



BOARD OF DIRECTORS OF THE EASTERN SIERRA TRANSIT AUTHORITY

Regular Meeting
Friday May 20, 2016
Town of Mammoth Lakes Council Chambers
Minaret Village Shopping Center, Suite Z
Mammoth Lakes, California
10:00 a.m.

Note: In compliance with the Americans with Disabilities Act, if an individual requires special assistance to participate in this meeting, please contact Eastern Sierra Transit at (760) 872-1901 or 800-922-1930. Notification 48 hours prior to the meeting will enable the Authority to make reasonable arrangements to ensure accessibility to this meeting. (28 CFR 13.102-35.104 ADA Title II)

- | | |
|--|---------------------------|
| 1. CALL TO ORDER | <u>DISPOSITION</u> |
| 2. PLEDGE OF ALLEGIANCE | |
| 3. ROLL CALL | |
| 4. NEW ESTA DIRECTOR | INFORMATION |
| Kirk Stapp, recently appointed by the Mono County Board of Supervisors to serve as an ESTA Director will be welcomed to the Board. | |
| 5. PUBLIC COMMENT | INFORMATION |
| 6. EMPLOYEE OF THE QUARTER PRESENTATION: (1 st Quarter 2016) | |
| 7. APPROVAL OF MINUTES | ACTION |
| Approval of minutes of the regular meeting of April 15, 2016 | |

8. REDS MEADOW ROAD REHABILITATION FUNDING

DISCUSSION/ POSSIBLE ACTION

Staff recommendation: Board direction is sought regarding possible participation in a plan to generate funds through a fare increase for ongoing maintenance of a new road rehabilitation project for the Reds Meadow Road.

9. 2016 SPECIAL USE PERMIT – REDS MEADOW SHUTTLE

ACTION

Staff recommendation: The Board is requested to approve the Special Use Permit from the U.S. Forest Service for the operation of the 2016 Reds Meadow Shuttle service, and to authorize the Executive Director to execute all associated documentation.

10. FINANCIAL REPORT

RECEIVE AND FILE

- a. FY 2015/16 report for the period ending May 17, 2016
- b. Year-end projection FY 2015/16

11. OPERATIONS REPORT

RECEIVE AND FILE

April 2016

12. REPORTS

INFORMATION

- a. Board Members
- b. Executive Director

13. FUTURE AGENDA ITEMS

14. CLOSED SESSION

DISCUSSION/POSSIBLE ACTION

Conference with Labor Negotiators. (Pursuant to Government Code Section 54957.6) – Authority designated representatives; John Helm, John Vallejo, Jim Tatum. Bargaining Group; ESTA-EEA.

15. ADJOURNMENT

ACTION

The next regular meeting is scheduled for June 17, 2016 at 11:30a.m. in the City of Bishop

EASTERN SIERRA TRANSIT AUTHORITY

Minutes of Friday April 15, 2016 Regular Meeting

The Board of Directors of the Eastern Sierra Transit Authority met at 10:00a.m. on Friday April 15, 2016 at the City of Bishop Council Chambers Bishop, California. The following members were present: Jo Bacon, Karen Schwartz, Mark Tillemans, Joe Pecsí and Tim Alpers. Director Jeff Griffiths arrived at 10:02am. Director Michael Raimondo was absent. Director Alpers led the pledge of allegiance.

Public Comment	The Chairperson opened the public comment period at 10:00a.m. There was no one from the public wishing to address the Board. Public comment was closed.
Approval of Minutes	<p>Moved by Director Tillemans and seconded by Director Pecsí to approve the minutes of the regular meeting of March 18, 2016. Motion carried 5-0, with Directors Raimondo and Griffiths absent.</p> <p>10:02a.m. Director Griffiths joined the meeting.</p>
Ticket Sales Agreement - 2016 Reds Meadow Shuttle	Mr. Helm presented the Ticket Sales Agreement with Mammoth Mountain Ski Area for the Reds Meadow Shuttle 2016. Moved by Director Alpers and seconded by Director Pecsí to approve the agreement with Mammoth Mountain Ski Area for Tickets Sales Agreement – 2016 Reds Meadow Shuttle, and authorized the Executive Director to execute the Agreement. Motion carried 6-0, with Director Raimondo absent.
Short Range Transit Plan Approval	<p>Mr. Helm presented the Short Range Transit Plan. Moved by Director Alpers and seconded by Director Griffiths to approve the Eastern Sierra Transit Authority Short Range Transit Plan dated April 13, 2016. Motion carried 6-0, with Director Raimondo absent.</p> <p>Directors Griffiths, Alpers and Pecsí requested Mr. Helm present ESTA’s Short Range Transit Plan to their respective Boards.</p>
Inter-City Route (5311f Funded) Grant Applications	Jill Batchelder updated the Board on the 5311F grant applications submitted this week, intended to expand service on the Reno and Lancaster routes.

Operations Report	Mr. Helm presented the Operations Report for the month of March 2016.
Financial Report	Mr. Helm presented the FY2015/16 Financial Report for the period ending April 12, 2016.
Board Member Reports	<p>Director Peksi reported he appreciates getting the background on ESTA.</p> <p>Director Griffiths reported he has spoken with staff at Cerro Coso College and they are pleased with the increased bus services at the college.</p> <p>Director Alpers reported he is pleased to announce the Mono Board of Supervisors will make an appointment to fill the empty seat on the ESTA Board. It is expected that Kirk Stapp will be appointed on Tuesday.</p>
Executive Director Report	Mr. Helm reported that the Operations Supervisor position in the Bishop office has been filled, and that progress is being made toward implementing Granicus (streaming and archiving of Board meetings) in the near future.
	10:59a.m. Director Griffiths left the meeting to attend another meeting.
Closed Session	Open session was recessed at 10:59a.m. to convene in closed session with all Board Members present except Directors Raimondo and Director Griffiths to consider Agenda Item #14 Conference with Labor Negotiators. (Pursuant to Government Code Section 54957.6) – Authority designated representatives; John Helm, John Vallejo and Jim Tatum. Bargaining Group; ESTA-EEA.
Report on Closed Session	Closed session was recessed at 11:56a.m. to reconvene in open session with all Board Members present except Directors Raimondo and Griffiths. There were no items discussed in closed session, which were required by law to be reported on in open session.
Adjournment	The Chairperson adjourned the meeting at 11:56a.m. The next regular meeting of the Eastern Sierra Transit Authority Board of Directors is scheduled for May 20, 2016 in the Town of Mammoth Lakes.

Recorded & Prepared by:

Susan Distel
Board Clerk
Eastern Sierra Transit Authority

Minutes approved:

STAFF REPORT

Subject: Reds Meadow Road Rehabilitation Funding

Initiated by: John Helm, Executive Director

BACKGROUND:

The U.S Forest Service and Federal Highways Administration were awarded a planning grant in 2014 to address rehabilitation of the road providing access to the Reds Meadow Valley, including Devils Postpile National Monument. As a result of this grant, the U.S. Forest Service and the Federal Highway Administration are currently conducting a Planning and Environmental Linkage (PEL) study to evaluation options to improve Reds Meadow Road.

ANALYSIS

The U.S. Forest Service and the Federal Highway Administration, in conjunction with the planning consultants, CH2M, who have been retained for the PEL study, have identified a number of options for the rehabilitation of the Reds Meadow Road. An overview of the options has been presented to a variety of stakeholders at meetings held this spring, and is included as an attachment with this agenda item. The options range in cost from approximately \$2.5m for minor improvements to the upper section of the roadway (replace culverts, add some turnouts), to \$29m for a significant overhaul to the entire roadway including making all of the upper portion 2-lane and major improvement of the lower section. A preferred option has not been selected at this point.

A major obstacle to implementation of any of the proposed options for improvements to the roadway is an ongoing maintenance plan and funding for the maintenance of the new, improved road. Inyo National Forest has incurred dramatic budget cuts in recent years and does not have sufficient ongoing operational funds to program the needed maintenance of the roadway. A variety of capital grants are available to the federal agencies (Inyo National Forest under the U.S. Forest Service, and Devils Postpile National Monument under the National Park Service) which it is believed can fund the capital requirements of the project. However, a key portion of any capital funding grant application is to have in place an ongoing maintenance plan, including the funding sources for the maintenance. According to the Forest Service engineers involved with the planning study, if the Inyo National Forest can partner with an entity to fund and/or provide at least 50% of the ongoing maintenance requirements of the

road rehabilitation capital project, the agency should be eligible for the capital funding.

Due to the challenge of the ongoing maintenance of the capital project, the Forest Service has looked to other entities for a creative solution to allow the grant to move forward. The Town of Mammoth Lakes has indicated that, in concept, the maintenance of the road could be provided by the Town under an arrangement with the Forest Service. Preliminary estimates have identified the cost of the road maintenance at \$1.4m over twenty years.

At a meeting held recently between representatives of the Inyo National Forest, Devils Postpile National Monument, the Town of Mammoth Lakes, and ESTA, an idea was discussed wherein additional funds to pay for the road maintenance would be developed through an increase in the Reds Meadow Shuttle ticket price. In essence, a road maintenance surcharge could be added to the fare for the shuttle ticket. Based upon historical ridership, the surcharge would need to be in the \$1.00 to \$1.50 range per ticket to fund the projected road maintenance costs. It is anticipated that, if ESTA desires to participate in funding of the Reds Meadow road maintenance through the imposition of a fare surcharge, such fare adjustment would be implemented beginning with the 2017 season.

Representatives from the Town of Mammoth Lakes and the Inyo National Forest will be available at the meeting to add clarification to the concept being discussed and to answer questions from the Board.

LEGAL

As designated in the ESTA Joint Powers Agreement, the Authority has the power to raise revenues, including the establishment of transportation fares, for transit services. The Annual Operating Plan of the Special Use Permit for the operation of the Reds Meadow Shuttle states that all adjustments to the Reds Meadow Shuttle fare structure shall be subject to review and approval by the Forest Service. Accordingly, with the concurrence of the Forest Service, which would be anticipated, the ESTA Board of Directors has the authority to modify the Reds Meadow Shuttle fare. Any modification of the Reds Meadow Shuttle fare would involve an active public participation process in compliance with ESTA's Title VI program.

FINANCIAL

Any adjustment to the Reds Meadow Shuttle fare in conjunction with an arrangement to help fund the maintenance of the road would be reflected in subsequent years' budgets. In general, such adjustment should be revenue-neutral, where increased revenues would be offset by increased expense to fund the road maintenance. It is anticipated that any increase to the fare for the Shuttle would reduce ridership for a period, although, it is difficult to

quantify the impact. The current fare structure of \$7.00 for adults and \$4.00 for children has been in place since at least 2008.

RECOMMENDATION

The Board is requested to provide direction regarding ESTA's possible involvement in helping to fund maintenance of the Reds Meadow road through fare revenues from the Reds Meadow Shuttle.



Reds Meadow Road PEL Study

CA FTFS 03S11 (1)

The US Forest Service (USFS) and the Federal Highway Administration, Central Federal Lands Highway Division (FHWA-CFLHD) are currently conducting a Planning and Environmental Linkage Study (PEL) to evaluate options to improve Reds Meadow Road, which provides access into Reds Meadow valley from State Route SR 203.

The purpose of the project is to improve the deteriorated condition of Reds Meadow Road and improve vehicular travel mobility. The roadway is deteriorated, and the steep one-lane roadway in the upper 2.5 miles hinders mobility because vehicles traveling in opposite directions cannot pass each other easily, resulting in long queues of waiting vehicles and safety concerns. Inadequate sight distance at curves and narrow shoulders also hinder passing and create safety risks. Without improvements, the roadway will continue to deteriorate and impede vehicular mobility.

As part of the PEL study and in coordination with the USFS, the project team has developed the purpose and need for the project as well as the initial range of conceptual alternatives and associated cost estimates for improvements along the approximately 8.3 mile roadway. Initial alternatives evaluated to date include primarily resurfacing improvements to the lower 5.8 miles with a combination of resurfacing and widening in the upper 2.5 miles. This information, along with a high level environmental review, will be used in conjunction with feedback received as part of the stakeholder meetings and agency coordination to help guide future efforts on the project.



Contacts

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Wendy Longley
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Wendy.Longley@dot.gov



Reds Meadow Road PEL Study Purpose and Need Statement

PREPARED FOR: Wendy Longley, FHWA-CFLHD
COPY TO: Timberly Belish, FHWA-CFLHD
PREPARED BY: CH2M
DATE: September 18, 2015
PROJECT NUMBER: FTFS 03S11 (1)

The purpose of the project is to improve the deteriorated condition of Reds Meadow Road and improve vehicular travel mobility. These improvements would facilitate emergency response in Reds Meadow valley and would facilitate the Forest Service goal of enhancing traffic safety by reducing the likelihood of a vehicular crash. Reds Meadow is a popular area for outdoor recreation, attracting nearly 100,000 visitors each season, and is important to the local tourism economy during the summer (Inyo National Forest, 2013). Reds Meadow Road provides the only access to the valley and Devils Post Pile National Monument, and the road is a popular access to two nationally significant trails: John Muir Trail and Pacific Crest Trail (see Figure 1).

The upper 2.5 miles of the 8-mile Reds Meadow Road is a paved one-lane roadway built on a combination of cut and fill as it descends into Reds Meadow valley. The original oiled dirt road was paved with approximately 2 inches of cold-mix asphalt in the early 1980s. Subsequent pavement maintenance has been limited to patching potholes and digging out small areas of settlement (CH2M, 2015). Pavement width of the one-lane road varies from 16 feet to 21 feet, with graded shoulders or roadside ditches varying in width from 1 foot to 5 feet. The lower 5.8 miles of roadway is a paved two-lane roadway with a relatively consistent pavement width of 22 feet. Graded shoulders or roadside ditches vary in width from 1 foot to 5 feet.

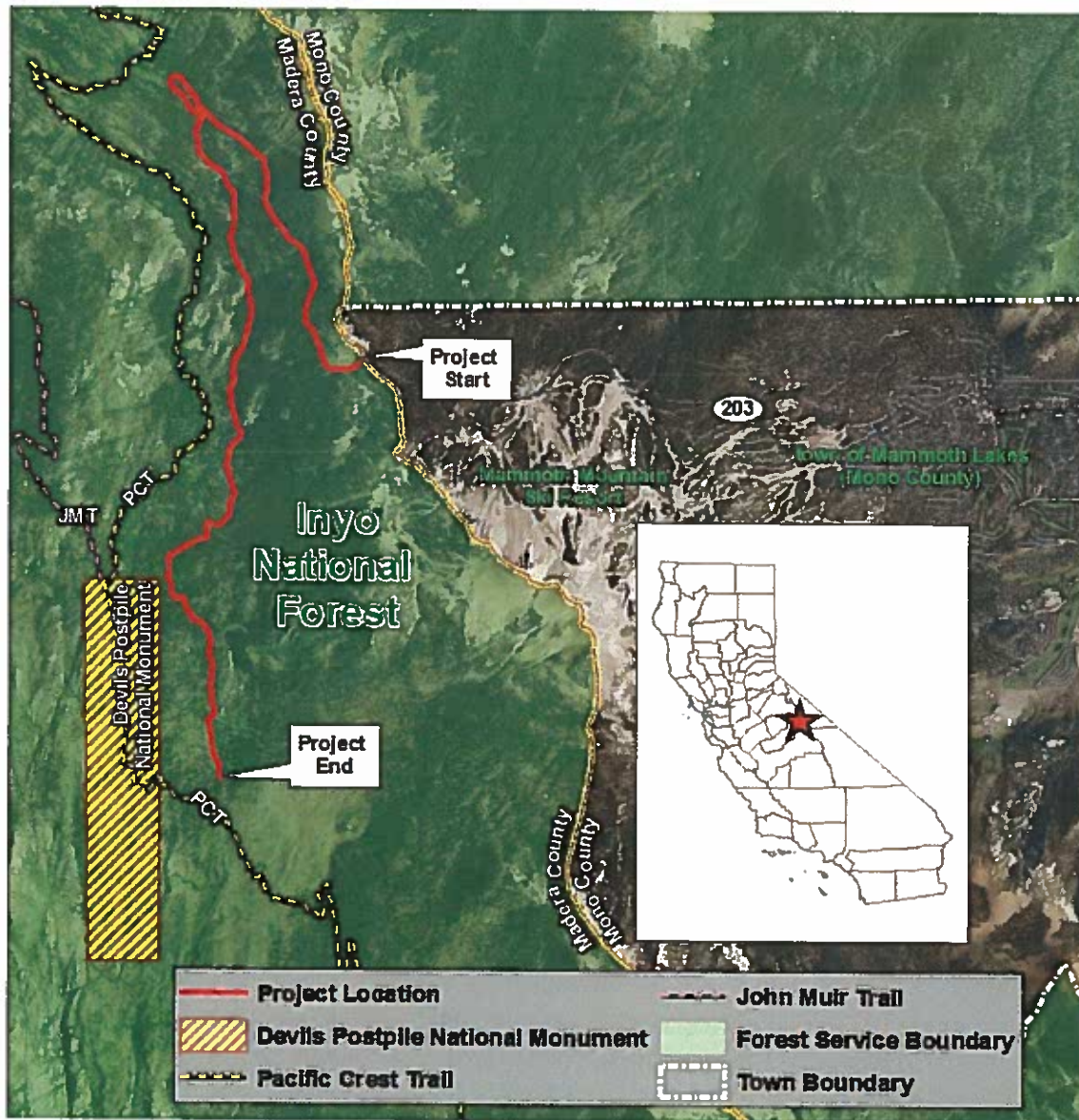
The roadway is deteriorated, with longitudinal cracks and edge deterioration along much of the upper 2.5 miles and in sections of the lower 5.8 miles that are supported by significant fill slopes. Road surface cracking also exists in several locations where the road crosses natural drainage swales. The cracks appear to be the result of fill settlement and slope creep, and the structural integrity of the fill slopes, particularly on the upper 2.5 miles, is questionable. The deteriorated condition of the roadway appears to be caused by lack of aggregate base layer, poor subgrade soil (e.g., pumice), settlement in poorly compacted fill areas, localized saturation from year-round runoff from springs, heavy traffic loading from frequent shuttle bus service, and lack of lateral support.

Approximately 450 shuttle buses and private and commercial vehicles travel the road each day (Inyo National Forest, 2013). The upper 2.5 miles of steep one-lane roadway hinders mobility because vehicles traveling in opposite directions cannot pass each other easily, resulting in long queues of waiting vehicles and safety concerns. Paved turnouts are located occasionally along the one-lane road segment but do not occur at regular intervals or in ideal locations for passing, relative to the road geometry. Inadequate sight distance at curves and narrow shoulders also hinder passing and create safety risks. Forest Service staff work continuously with shuttle bus drivers to manage traffic, often holding vehicles at the bottom of the valley behind a shuttle bus or at the top of the valley at the entrance station to allow queues of vehicles to safely pass. The narrow one-lane roadway additionally

inhibits quick emergency service access into the valley and concurrent evacuation of visitors out of the valley in an earthquake or fire event.

Without improvements, the roadway will continue to deteriorate and impede vehicular mobility. Maintenance activities provide only temporary roadway repairs and cannot address ongoing structural and drainage concerns. Traffic management by Forest Service staff allows for safer vehicle passage but does not address inefficient mobility in and out of the valley. The one-lane roadway section will continue to pose safety concerns for emergency response and evacuation in the valley and will perpetuate the potential for vehicle accidents.

Figure 1 – Project Location

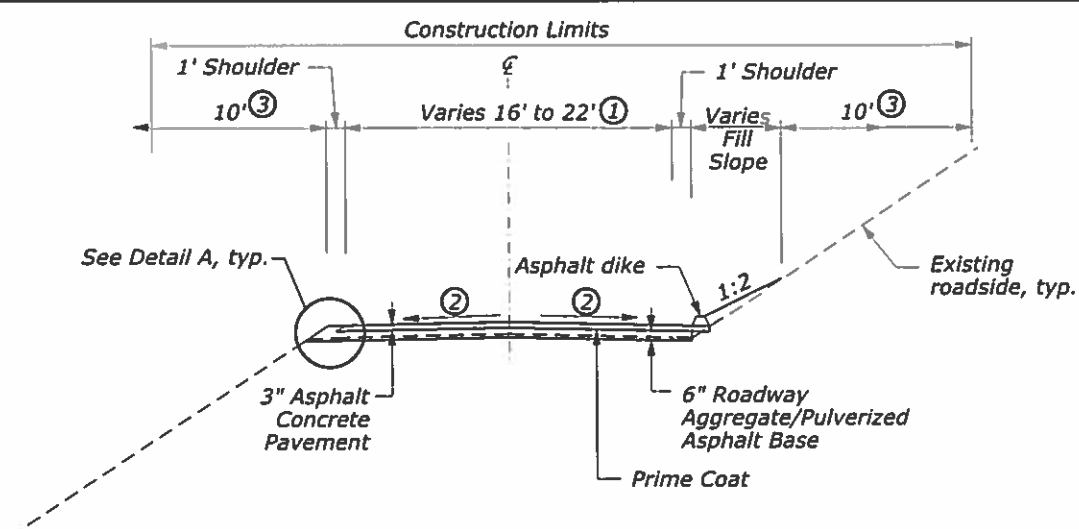


References

CH2M. 2015. Interview with Inyo National Forest staff. June 18.

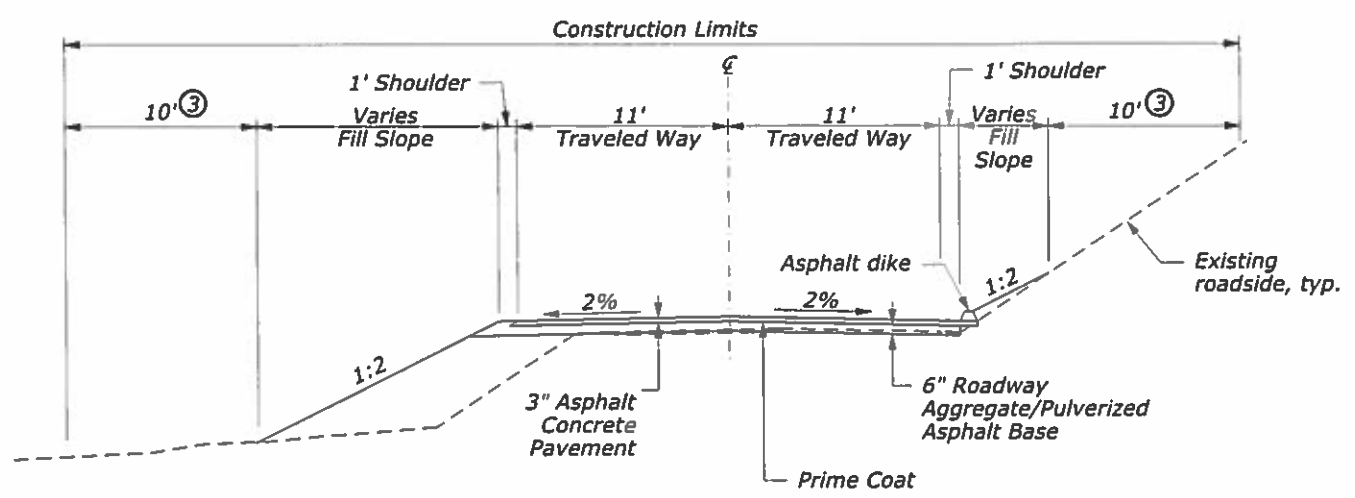
Inyo National Forest. 2014. *Inyo National Forest Alternative Transportation System Study*. December 15.

REG	STATE	PROJECT	SHEET NO.	TOTAL SHEETS
N/A	CA	FTFS 03511 (1)	A3	A3



**PAVEMENT RECONSTRUCTION TYPICAL SECTION
REDS MEADOW ROAD**

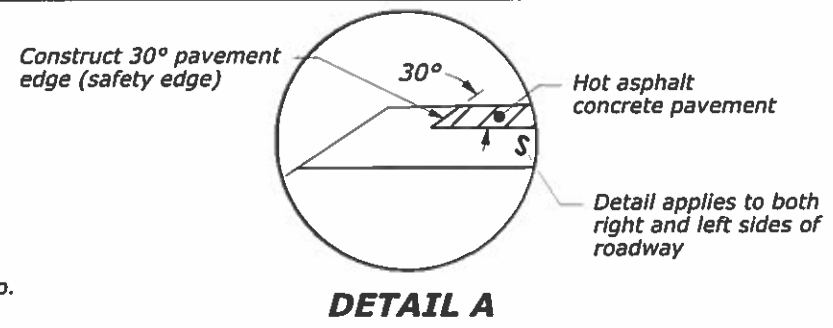
- ALTERNATIVE 1 9+00.00 TO 446+30.00
- ALTERNATIVE 2 9+00.00 TO 446+30.00 ⑥
- ALTERNATIVE 3 9+00.00 TO 11+00.00
- ALTERNATIVE 3 138+00.00 TO 446+30.00



**ROAD WIDENING (FILL) TYPICAL SECTION
REDS MEADOW ROAD**

- ALTERNATIVE 2 21+00.00 TO 134+00.00 ④
- ALTERNATIVE 3 11+00.00 TO 138+00.00 ④

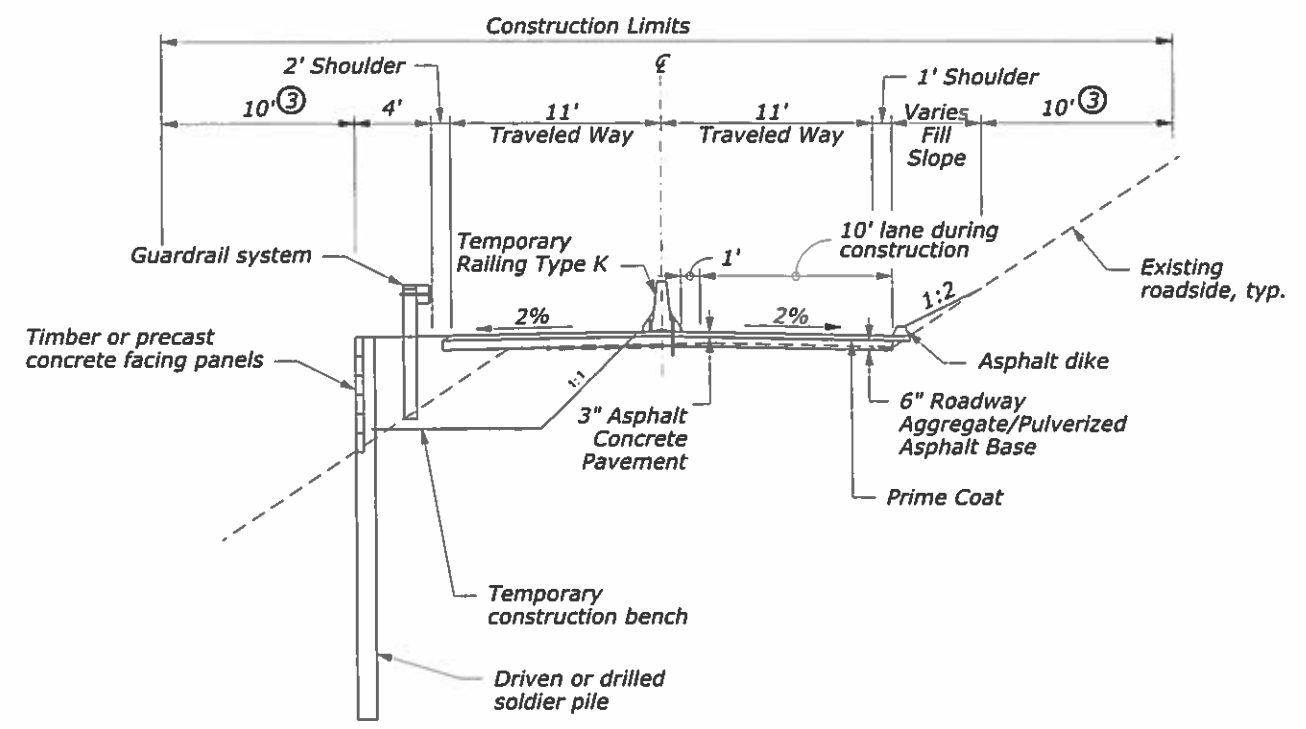
LENGTH OF PROJECT				
Location	Station to Station	Roadway (ft)		
		Alternative 1	Alternative 2	Alternative 3
Reds Meadow Road	9+00.00 to 446+30.00	43,730	43,730	43,730
	TOTAL (mi)	8.28	8.28	8.28



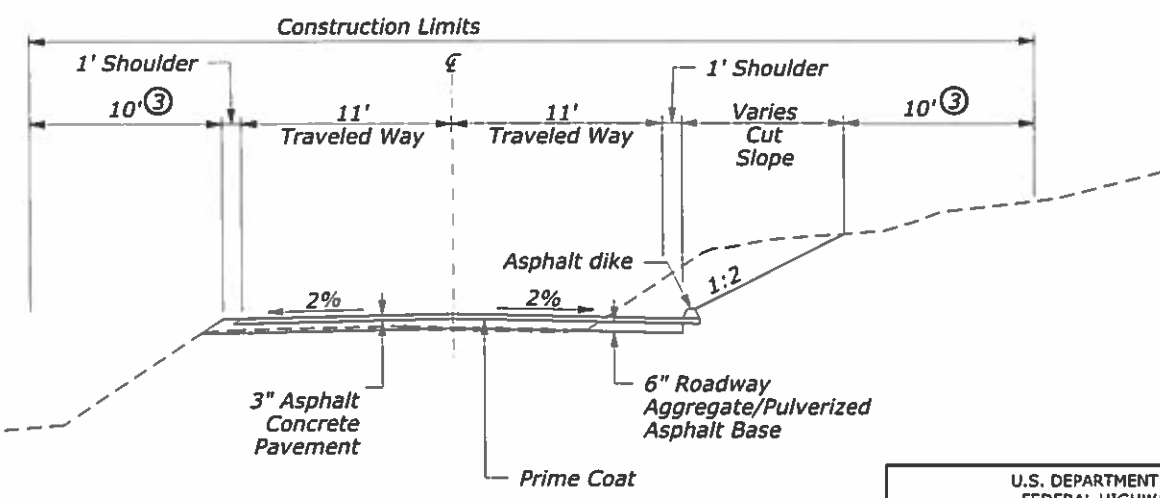
DETAIL A

NOTE:

- ① Match existing pavement width.
- ② Match existing cross slopes.
- ③ The contractor shall not remove vegetation or topsoil within the 10' wide area and shall only use this area for temporary construction access.
- ④ See C series for locations.
- ⑤ Where subgrade soils are composed of pumice, they should be sub-excavated 3-feet, reinforced with geotextile fabric and recompacted. See C-series sheets for approximate locations.
- ⑥ Except where road widening fill or cut is proposed, see Sheet C-1.



FILL WALL TYPICAL SECTION



**ROAD WIDENING (CUT) TYPICAL SECTION
REDS MEADOW ROAD**

- ALTERNATIVE 2 12+00.00 TO 17+00.00
- ALTERNATIVE 3 11+00.00 TO 138+00.00 ④

NO SCALE

U.S. DEPARTMENT OF TRANSPORTATION
FEDERAL HIGHWAY ADMINISTRATION
CENTRAL FEDERAL LANDS HIGHWAY DIVISION

TYPICAL SECTIONS

Sheet 1 of 1

12/7/2015 4:30:01 PM p:\projectwise.ch2m.com\DEN001\Documents\663395 - FHWA-TO 27 REDS MEADOW FEL\Work in Progress\Reds Meadow Road WJPI Roadway\CADD_Sheets\A-Gen_sht\A03_TypeRM_01.dwg

CONCEPTUAL DESIGN SUBMITTAL - DECEMBER 11, 2015 - NOT FOR CONSTRUCTION

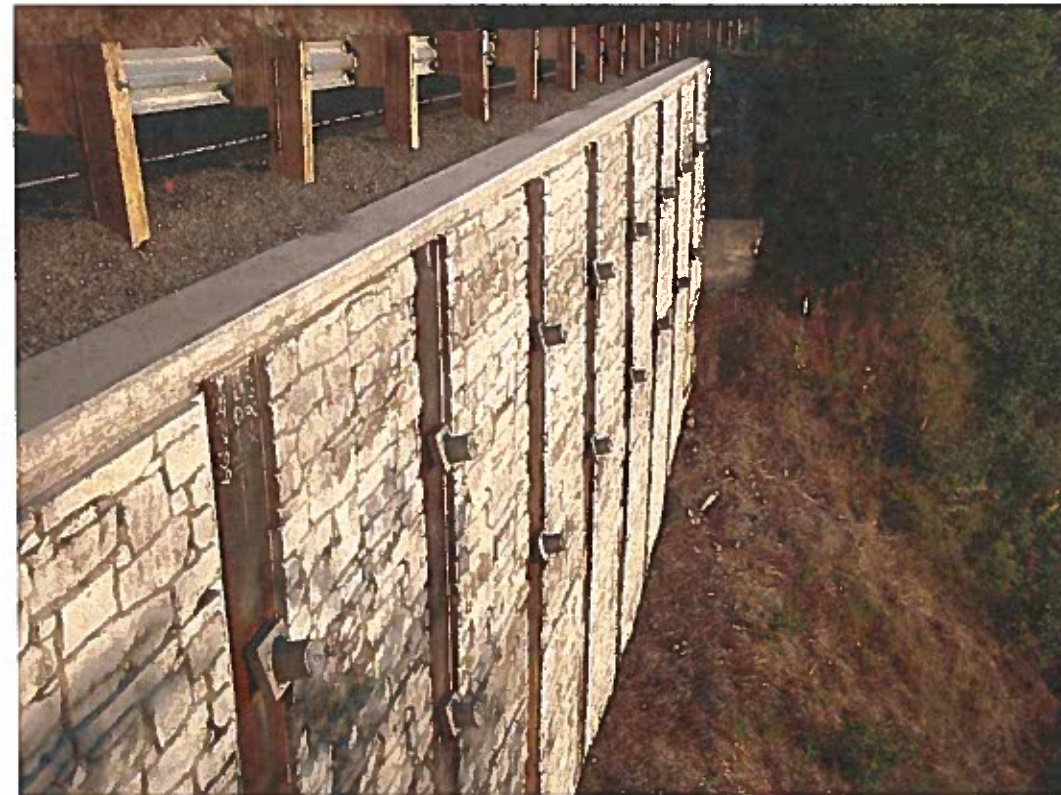
Reds Meadow Road PEL Study Stakeholder Meetings – Example Wall Types



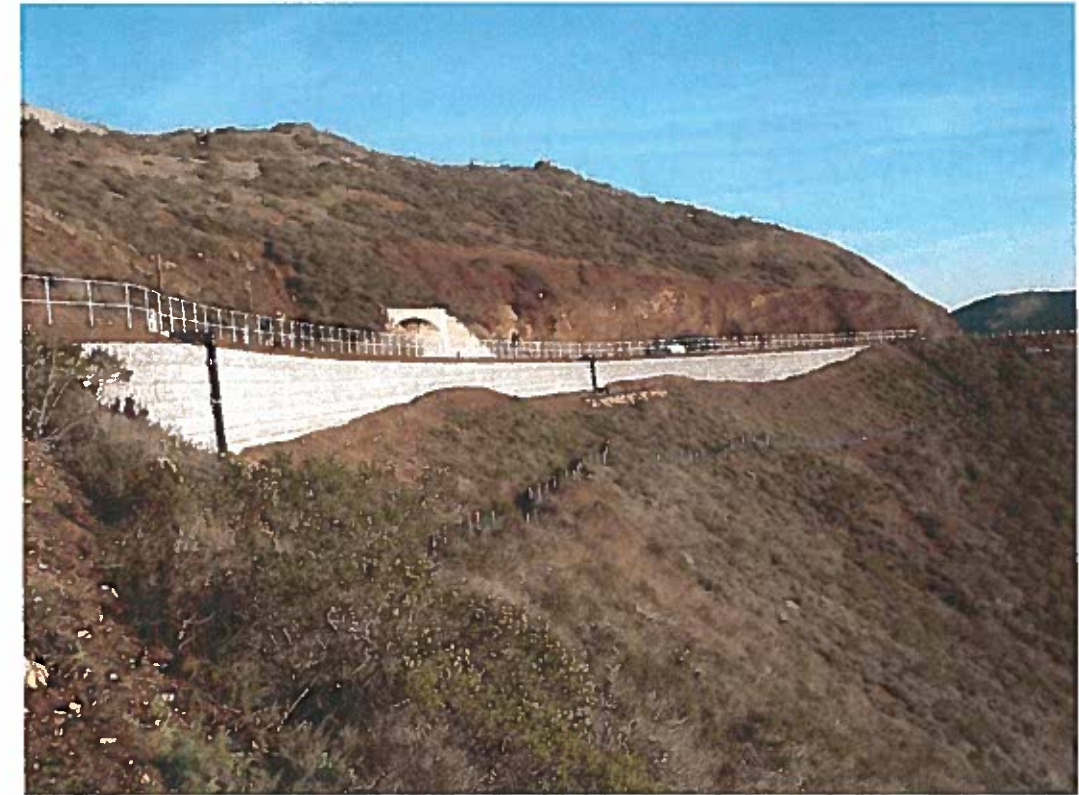
Photograph 1. Rockery Wall



Photograph 2. Cast in Place Concrete Wall



Photograph 3. Tie Back Wall



Photograph 4. Mechanically Stabilized Embankment Wall



Photograph 5. Rockery Wall



Photograph 6. Soldier Pile Wall with Timber Lagging



Photograph 7. Cast in Place Concrete Wall with Aesthetic Treatment

Screening Criteria	No Build	3R Entire Route	No Build Lower, Combo 1-/2-Lane Upper	No Build Lower, 2-Lane Upper	3R Lower, Combo 1-/2-Lane Upper	3R Lower, 2-Lane Upper	New Alignment Roadway	New Alignment Emergency Access Route
Fulfill Purpose and Need - Address roadway deterioration								
To what degree does the alternative improve roadway drainage?	No improvement.	Improvements limited to culvert replacement only - adequate culvert size, reduced risk of failure.	Upper: Improvements limited to culvert replacement only - adequate culvert size, reduced risk of failure. Lower: No improvement.	Upper: Substantial improvement due to eliminating existing roadside swales - reduces subgrade infiltration. Lower: No improvement.	Improvements limited to culvert replacement only - adequate culvert size, reduced risk of failure.	Substantial improvement due to eliminating existing roadside swales on upper section and reduced subgrade infiltration; and culvert replacement on lower section - adequate culvert size, reduced risk of failure.	New roadway would eliminate any existing drainage concerns.	Although new emergency route has appropriate drainage design, there would be no improvement on existing road.
To what degree does the alternative improve the existing poor subgrade conditions?	No improvement.	Minor improvement due to recompaction of subgrade as part of the repaving process but no excavation of poor soils.	Upper: Minor improvement due to recompaction of subgrade but no excavation of poor soils. Lower: No improvement.	Upper: Substantial improvement as retaining wall construction would allow for excavation of poor soils. Lower: No improvement.	Minor improvement due to recompaction of subgrade as part of the repaving process but no excavation of poor soils.	Upper: Substantial improvement due to retaining wall excavation and backfill. Lower: Minor improvement due to recompaction of subgrade.	New roadway has appropriate subgrade conditions.	New emergency route has appropriate subgrade conditions. No improvement on existing road.
To what degree does the alternative improve slope stability?	No improvement.	No improvement because slopes won't be modified, except one location on lower section.	Upper: Improvements limited to areas of new turnouts and retaining walls. Lower: No improvement.	Upper: Full improvement due to retaining wall excavation and backfill. Lower: No improvement.	Upper: Improvements limited to areas of new turnouts and retaining walls. Lower: No improvement, except one location.	Upper: Full improvement due to retaining wall excavation and backfill. Lower: No improvement, except one location.	New roadway has appropriate slope stability.	New emergency route has appropriate slope stability. No improvement on existing road.
Fulfill Purpose and Need - Improve mobility								
To what degree does the alternative improve passing conditions on the upper section of roadway?	No improvement.	No improvement.	Improvements limited to additional turnout locations only, remainder of upper section would not improve.	Substantial improvement as 2-lane roadway would provide adequate passing width along entire section.	Improvements limited to additional turnout locations only, remainder of upper section would not improve.	Substantial improvement as 2-lane roadway would provide adequate passing width along entire section.	New roadway has 2 lanes, providing 2-way travel.	No improvement to existing roadway.
To what degree does the alternative provide safer travel conditions for vehicles?	No improvement.	Improves conditions with new roadway surface. Minimal change to safety because no change in pavement width, passing conditions, curve safety, or sight distance.	Upper: Moderate improvement to safety due to additional turnouts and improved surface conditions, but no changes to curve safety or sight distance. Lower: No improvement.	Upper: Substantial improvement to safety by allowing two-way traffic along full length of upper section coupled with new roadway surface. Lower: No improvement.	Upper: Moderate improvement to safety due to additional turnouts and improved surface conditions, but no changes to curve safety or sight distance. Lower: Improves surface conditions, but no other changes to safety.	Upper: Substantial improvement to safety by allowing two-way traffic along full length of upper section coupled with new roadway surface. Lower: Improves surface conditions, but no other changes to safety.	Provides the best improvement to safety because new roadway is designed and built to current standards.	No improvement to existing roadway.
To what degree does the alternative improve access for incoming emergency responders and outgoing evacuees?	No improvement.	Improves surface conditions only and likely results only minimal change in emergency response access.	Minor improvement due to additional turnouts providing more opportunities for emergency responders to pass through traffic.	Substantial improvement due to two-way travel for entire roadway length.	Minor improvement due to additional turnouts providing more opportunities for emergency responders to pass through traffic.	Substantial improvement due to two-way travel for entire roadway length.	Substantial improvement because new roadway provides two-way travel and is designed and built to current standards.	Substantial improvement because emergency access route provides dedicated route for emergency responders with no interaction with visitors on existing route.

Screening Criteria	No Build	3R Entire Route	No Build Lower, Combo 1-/2-Lane Upper	No Build Lower, 2-Lane Upper	3R Lower, Combo 1-/2-Lane Upper	3R Lower, 2-Lane Upper	New Alignment Roadway	New Alignment Emergency Access Route
Constructability								
What is the complexity, difficulty, and duration of construction?	NA	Standard construction methods for 3R. Anticipated construction duration limited to one season.	Standard construction methods for 3R. Anticipated construction duration limited to one season.	Increased difficulty of construction to build retaining wall systems and improve roadway width and subgrade on upper section due to increased roadway width. As a result, anticipated construction duration likely to extend through two seasons.	Standard construction methods for 3R. Anticipated construction duration limited to one season.	Increased difficulty of construction to build retaining wall systems and improve roadway width and subgrade on upper section due to increased roadway width. As a result, anticipated construction duration likely to extend through two seasons.	Increased difficulty of construction to build roadway on entirely new alignment. Extended construction duration, but would not affect existing roadway operation. As a result, anticipated construction duration likely to extend through two seasons.	Increased difficulty of construction to build roadway on entirely new alignment. Extended construction duration, but would not affect existing roadway operation. As a result, anticipated construction duration likely to extend through two seasons.
How does the alternative maintain visitor and emergency access to the valley during construction?	NA	Access maintained via standard traffic control using flaggers to direct one-way traffic during work hours. Regular traffic flow outside of work hours.	Access maintained via standard traffic control using flaggers to direct one-way traffic during work hours. Regular traffic flow outside of work hours. Lower: No construction, full access maintained.	Upper: Access maintained via complex traffic control due to work areas needed for retaining wall construction; requires installation of temporary K-rail and operation of one-way traffic signal system and minor traffic delays around the clock. Lower: No construction, full access maintained.	Access maintained via standard traffic control using flaggers to direct one-way traffic during work hours. Regular traffic flow outside of work hours.	Upper: Access maintained via complex traffic control due to work areas needed for retaining wall construction; requires installation of temporary K-rail and operation of one-way traffic signal system and minor traffic delays around the clock. Lower: Access maintained via standard traffic control using flaggers to direct one-way traffic during work hours.	No traffic control needed.	No traffic control needed.
Is construction of the alternative financially feasible?	Estimated construction cost: \$0	Estimated construction cost: \$7M	Estimated construction cost: \$2.5M	Estimated construction cost: \$24M	Estimated construction cost: \$7.5M	Estimated construction cost: \$29M	Estimated construction cost: Unknown	Estimated construction cost: Unknown
Long-Term Operations and Maintenance								
Does the alternative have the potential to improve operations and maintenance?	No potential for improvement.	Potential for moderate improvement due to improved pavement structural section and new culverts, thereby reducing maintenance costs and improving rideability.	Upper: Potential for moderate improvement due to improved pavement structural section and new culverts. Lower: No potential for improvement, existing yearly maintenance costs would continue to increase.	Upper: Potential for substantial improvement due to fully addressing slopes, subgrade, and drainage conditions and ability to easily conduct maintenance operations and maintain traffic on 2-lane road. Lower: No potential for improvement, existing yearly maintenance costs would continue to increase.	Potential for moderate improvement due to improved pavement structural section and new culverts, thereby reducing maintenance costs and improving rideability.	Upper: Potential for substantial improvement due to fully addressing slopes, subgrade, and drainage conditions and ability to easily conduct maintenance operations and maintain traffic on 2-lane road. Lower: Potential for moderate improvement due to improved pavement structural section and new culverts.	Potential for substantial improvement because new road would be constructed to current standards.	Potential for substantial improvement on new emergency route because road would be constructed to current standards. No potential for improvement on existing road.

Screening Criteria	No Build	3R Entire Route	No Build Lower, Combo 1-/2-Lane Upper	No Build Lower, 2-Lane Upper	3R Lower, Combo 1-/2-Lane Upper	3R Lower, 2-Lane Upper	New Alignment Roadway	New Alignment Emergency Access Route
Community Values								
How does the construction duration and/or closure plans impact businesses and recreation?	NA	Single season construction duration likely to have limited impact on visitation compared to 2-lane upper alternatives.	Single season construction duration likely to have limited impact on visitation compared to 2-lane upper alternatives.	Longer construction duration likely to have more impact on visitation than other alternatives except 3R Lower, 2 Lane Upper.	Single season construction duration likely to have limited impact on visitation compared to 2-lane upper alternatives.	Longer construction duration likely to have more impact on visitation than other alternatives.	Construction duration not likely to impact visitation because does not affect existing road operation.	Construction duration not likely to impact visitation because does not affect existing road operation.
Is the alternative compatible with established local plans and visions?	Yes, will continue to allow visitation to valley.	Yes, will continue to allow visitation to valley during both construction and operation.	Yes, will continue to allow visitation to valley during both construction and operation.	Yes, will continue to allow visitation to valley during both construction and operation.	Yes, will continue to allow visitation to valley during both construction and operation.	Yes, will continue to allow visitation to valley during both construction and operation.	Yes, will continue to allow visitation to valley during both construction and operation.	Yes, will continue to allow visitation to valley during both construction and operation.
Environmental Resources								
How much previously undisturbed land does the alternative impact?	0 acres	0 acres	3 acres	8 acres	4 acres	9 acres	30 acres	15 acres
How does the alternative impact wetlands?	No impact.	Negligible wetland impact limited to culvert replacement only.	Some wetland impact likely, due to culvert replacement.	Some wetland impact likely, due to wider cross section in upper section.	Some wetland impact likely, due to culvert replacement.	Some wetland impact likely, due to culvert replacement in lower section and wider cross section in upper section.	Greatest potential for wetland impact due to new alignment extending through previously undisturbed areas.	Greatest potential for wetland impact due to new alignment extending through previously undisturbed areas.
How does the alternative impact sensitive plant and animal species?	No impact.	Least potential for impact on sensitive animal species due to light and noise from construction activities. No impact on critical habitat or migratory birds.	Minor potential impact on sensitive animal species due to light and noise from construction activities. Less potential for impact to sensitive plant species than 2-lane alternatives because of smaller footprint. Minor potential for impact to migratory birds due to vegetation removal.	Moderate potential impact on sensitive animal species due to light and noise from long duration of construction activities in a larger construction footprint than alternatives without 2 lanes. Higher potential for impact to sensitive plant species due to larger footprint than other alternatives. Minor potential for impact to migratory birds due to vegetation removal.	Minor potential impact on sensitive animal species due to light and noise from construction activities. Less potential for impact to sensitive plant species than 2-lane alternatives because of smaller footprint. Minor potential for impact to migratory birds due to vegetation removal.	Moderate potential impact on sensitive animal species due to light and noise from long duration of construction activities in a larger construction footprint than alternatives without 2 lanes. Higher potential for impact to sensitive plant species due to larger footprint than other alternatives. Minor potential for impact to migratory birds due to tree removal.	Greatest potential impact on sensitive animal species due to light and noise from long duration of construction activities in a larger construction footprint than other alternatives. Highest potential for impact to sensitive plant species due to new alignment construction. Moderate potential for impact to migratory birds due to tree removal.	Greatest potential impact on sensitive animal species due to light and noise from long duration of construction activities in a larger construction footprint than most other alternatives. Highest potential for impact to sensitive plant species due to new alignment construction. Moderate potential for impact to migratory birds due to tree removal.
How does the alternative impact cultural and/or tribal resources?	No impact.	Negligible potential for impact due to limited disturbance.	Negligible potential for impact due to limited disturbance.	Negligible potential for impact due to limited disturbance.	Negligible potential for impact due to limited disturbance.	Negligible potential for impact due to limited disturbance. Roadway maintains current alignment.	Greatest potential for wetland impact due to new alignment extending through previously undisturbed areas.	Greatest potential for wetland impact due to new alignment extending through previously undisturbed areas.
What visual impacts does the alternative have?	No impact.	No visual impact because no change to roadway width or alignment.	Although the addition of retaining walls and tree removal in areas of new pullouts may result in limited visual changes; there are few views of the road from the valley or nearby trails.	Although the addition of retaining walls and tree removal in areas of new pullouts may result in limited visual changes; there are few views of the road from the valley or nearby trails.	Although the addition of retaining walls and tree removal in areas of new pullouts may result in limited visual changes; there are few views of the road from the valley or nearby trails.	Although the addition of retaining walls and tree removal in areas of new pullouts may result in limited visual changes; there are few views of the road from the valley or nearby trails.	Greatest potential for visual impact due to construction of new roadway and associated tree removal and cut and fill slopes.	Greatest potential for visual impact due to construction of new roadway and associated tree removal and cut and fill slopes.

Screening Criteria	No Build	3R Entire Route	No Build Lower, Combo 1-/2-Lane Upper	No Build Lower, 2-Lane Upper	3R Lower, Combo 1-/2-Lane Upper	3R Lower, 2-Lane Upper	New Alignment Roadway	New Alignment Emergency Access Route
How does the alternative impact designated wilderness and other sensitive areas?	No impact.	No impact because no change in roadway alignment.	No impact because no change in roadway alignment.	No impact because no change in roadway alignment.	No impact because no change in roadway alignment.	No impact because no change in roadway alignment.	Greatest potential for wetland impact due to new alignment extending through previously undisturbed areas.	Greatest potential for wetland impact due to new alignment extending through previously undisturbed areas.

STAFF REPORT

Subject: Reds Meadow Shuttle Special Use Permits
Initiated by: John Helm, Executive Director

BACKGROUND:

ESTA has operated or subcontracted the operation of the Reds Meadow shuttle service since 2009 through either a Challenge Cost Share Agreement or, beginning in 2012, a Special Use Permit with the U.S. Forest Service.

ANALYSIS/DISCUSSION:

ESTA's operation of the Reds Meadow shuttle since 2012 has been governed by a Special Use Permit that is issued for each summer season's operation. The Forest Service issues a one-season permit to ascertain if the operation under a special use permit is appropriate and to defer the completion of required regulatory approvals necessary for a longer term permit. ESTA and the Forest Service have been exploring the possibility of a longer term permit over the past few years, however, the Forest Service continues to believe that the term Special Use Permit valid for one season's operation is the appropriate governing document for the service.

The Special Use Permit for the 2016 Reds Meadow season is based on, and includes all material aspects of the 2015 Permit. It is expected that the term of the Permit shall not exceed 180 days in length and will expire no later than December 1, 2016. Other provisions from previous years that will be incorporated in the 2016 Permit include:

- Non-exclusive use: Other uses that do not materially interfere with ESTA's authorized use will be permitted.
- Description of applicable annual fees (3% of gross revenue)
- Requirement for Forest Service review of proposed fare pricing
- Requirement for submittal of annual operating plan

The proposed 2016 Special Use Permit and associated Annual Operating Plan are included for review by the Board. John Vallejo, Mono County Deputy Counsel, has previously reviewed the U.S. Forest Service Special Use Permit.

A copy of the 2015 season Special Use Permit is attached for the Board's information. The 2016 season Special Use Permit is expected to mirror, in all material aspects, the 2015 Special Use Permit. The 2016 Special Use Permit application and the 2016 season Annual Operating Plan are also attached for the Board's review.

FINANCIAL CONSIDERATIONS

The operation of the Reds Meadow Shuttle service under a Forest Service Special Use Permit allows the Authority to apply excess program income to the Authority's fund balance in order to strengthen the Authority's financial position overall and to provide a financial cushion for the operation of the Shuttle service. In FY 2015/16, it is projected that the Reds Meadow Shuttle will generate \$490,000 in revenues. The revenues and expenses for the 2016 Reds Meadow Shuttle service will be included in the FY 2016/17 Budget.

RECOMMENDATION

The Board is requested to authorize the Eastern Sierra Transit Authority to enter into a Special Use Permit to be issued by the U.S. Forest Service for the operation of the Reds Meadow Shuttle service in 2016, and to authorize the Executive Director to execute all required documents for the Permit.

Use Code:		FS-2700-3f (10/09)
Authorization ID:	FOREST SERVICE USE	OMB No. 0596-0082
Contact Name:		
Expiration Date:		

SPECIAL USE APPLICATION & TEMPORARY PERMIT FOR OUTFITTING AND GUIDING
Authority: Federal Lands Recreation Enhancement Act, 16 U.S.C. 6802(h)
(Ref.: FSH 2709.11, section 41.53)

PART I - APPLICATION

1. APPLICANT INFORMATION

Applicant Name: EASTERN SIERRA TRANSIT AUTHORITY

Business Name: EASTERN SIERRA TRANSIT AUTHORITY

Applicant's Complete Address: P.O. Box 1357 (703B Airport Road), Bishop CA 93515

Telephone Number: 760.872.1901 Fax Number: 760.872.0936

E-mail Address: jhelm@estransit.com

Website: www.estransit.com

As an applicant, are you:

<input type="checkbox"/> Individual	If yes, are you a citizen of the United States?
<input type="checkbox"/> Corporation	If yes, provide a copy of your state certificate of good standing.
<input type="checkbox"/> Limited Liability Company	If yes, provide a copy of your state certificate of good standing.
<input type="checkbox"/> Partnership or Association	If yes, provide a copy of your partnership or association agreement.
<input type="checkbox"/> State Government or Agency	(Includes state universities)
<input checked="" type="checkbox"/> Local Government or Agency	(Includes high schools)
<input type="checkbox"/> Nonprofit	(Please attach a copy of your IRS Form 990)

Under the Regulatory Flexibility Act, a small entity is a firm that is "independently owned and operated" and "not dominant in its field of operation." The United States Small Business Administration has developed size standards to identify what is considered a small business. Under these standards, a business with annual receipts of less than \$6.5 million constitutes a small business for recreation industries. Additionally, a small organization is any nonprofit enterprise that is independently owned and operated and not dominant in its field. A small

government jurisdiction is a government of a city, county, town, township, village, school district, or special district with a population of less than 50,000.

Under these criteria, are you a small entity? **YES**

**2. DESCRIPTION OF PROPOSED ACTIVITY: OPERATION OF THE REDS MEADOW SHUTTLE
SEE ATTACHED ANNUAL OPERATING PLAN**

Please include:

- . The number of service days requested (or quota equivalent).
- . The anticipated number of trips and party size.
- . Trip Itinerary with:
 - . Starting and ending dates of the proposed operations.
 - . Location of routes and starting and ending points for the proposed operations (include a map showing these locations).
 - . Services that will be offered to clients (identify any services that will be provided by a party other than the holder).
 - . A description of your client base or audience.
 - . A list of government facilities you propose to use, e.g., a boat launch, parking lot, or trailhead.
 - . A list of temporary improvements or signs that you propose to use.
 - . A statement of whether the proposed operations involve motorized equipment.
 - . A statement of whether the proposed operations involve transportation livestock, and if so, whether grazing is requested.
 - . A statement of whether an assigned site is requested.
 - . A description of cleanup and restoration during and after the proposed operations.

3. ADVERTISING. Provide a current brochure and current advertising materials or website address.
SEE ATTACHED ANNUAL OPERATING PLAN

4. CLIENT CHARGES. Provide a description of client charges and fees and what they cover. Attach a current rate sheet. **SEE ATTACHED ANNUAL OPERATING PLAN**

5. GUIDE IDENTIFICATION

- . Attach a list of all guides who would be working under the permit.
- . Describe your requirements for employment and staff training programs.
- . Attach copies of current CPR and First Aid certifications, Wilderness First Responder cards, and other applicable certifications for guides. Please do not send copies of social security cards or passports. Send driver's licenses only if driving is part of the outfitting and guiding service.
- . If the state in which your activity would occur requires licensing for outfitters and guides, include a copy of relevant licenses.

N/A

6. OPERATING PLAN. Attach an operating plan that addresses client and visitor safety, evacuation and emergency procedures, and resource protection with respect to your proposed operations and location.
SEE ATTACHED ANNUAL OPERATING PLAN

7. LIABILITY INSURANCE. The holder will be required to obtain liability insurance in an amount satisfactory to the authorized officer (see FSM 2713.1). The insurance policy must name the United States as an additional insured. A copy of the certificate of insurance must be provided to the authorized officer prior to issuance of a permit.

8. CLIENT'S ACKNOWLEDGMENT OF RISK FORM. If you plan to use an acknowledgment of risk form, attach a copy.

9. EXPERIENCE. List all permits for outfitting and guiding on National Forest System lands that you have held in the past 3 years. If you received a performance evaluation from the Forest Service, attach a copy. If you are relying on outfitting and guiding experience with other federal or state agencies, list any permits that you have held with those agencies in the past 3 years and provide a copy of any performance evaluations received. List all citations or violations received in association with outfitting and guiding activities.

ESTA has operated the Reds Meadow Shuttle service continuously under a Special Use Permit each summer season since 2012.

10. SIGNATURE. I hereby certify that I am of legal age and am authorized to do business in the State of California. I have personally examined the information contained in this application and certify that this information is correct to the best of my knowledge. I hereby acknowledge that this is an application only, and that the use and occupancy of National Forest System lands is not authorized until a special use permit is signed and issued by an authorized officer.

Printed Name: **John Helm** Signature: _____ Date: _____

According to the Paperwork Reduction Act of 1995, an agency may not conduct or sponsor, and a person is not required to respond to a collection of information unless it displays a valid OMB control number. The valid OMB control number for this information collection is 0596-0082. The time required to complete this information collection is estimated to average 4 hours per response, including the time for reviewing instructions, searching existing data sources, gathering and maintaining the data needed, and completing and reviewing the collection of information.

The U.S. Department of Agriculture (USDA) prohibits discrimination in all its programs and activities on the basis of race, color, national origin, age, disability, and where applicable, sex, marital status, familial status, parental status, religion, sexual orientation, genetic information, political beliefs, reprisal, or because all or part of an individual's income is derived from any public assistance. (Not all prohibited bases apply to all programs.) Persons with disabilities who require alternative means for communication of program information (Braille, large print, audiotape, etc.) should contact USDA's TARGET Center at 202-720-2600 (voice and TDD).

To file a complaint of discrimination, write USDA, Director, Office of Civil Rights, 1400 Independence Avenue, SW, Washington, DC 20250-9410 or call toll free (866) 632-9992 (voice). TDD users can contact USDA through local relay or the Federal relay at (800) 877-8339 (TDD) or (866) 377-8642 (relay voice). USDA is an equal opportunity provider and employer.

The Privacy Act of 1974 (5 U.S.C. 552a) and the Freedom of Information Act (5 U.S.C. 552) govern the confidentiality to be provided for information received by the Forest Service.

Appendix A - 2016 ANNUAL OPERATING PLAN
Eastern Sierra Transit Authority, Reds Meadow Shuttle Service

I. GENERAL OVERVIEW OF OPERATIONS

This operating plan outlines the expectations and requirements of Eastern Sierra Transit Authority (ESTA), the Inyo National Forest and the Devils Postpile National Monument National Park Service for the Reds Meadow Shuttle service.

1. Shuttle Fees

a. Shuttle fares are as follows:

Adult (over 15 years old) daily pass:	\$7.00
Child (aged 3 – 15 years old) daily pass:	\$4.00
Infant (< 2 years old):	no charge
Adult 3-day pass (good for 3 out of 5 days):	\$14.00
Child 3-day pass (good for 3 out of 5 days):	\$8.00
Adult season pass (good for all of 2016):	\$35.00
Child season pass (good for all of 2016):	\$20.00
Dog:	no charge

b. ESTA or its surrogate will collect passenger fares during all hours of shuttle operations and provide sufficient staffing and/or ticket vending machines to minimize passenger wait time for ticket purchase.

i. ESTA, or its surrogate, will sell shuttle tickets from a dedicated ticket sales window at the Adventure Center or bus loading area from 8:00a.m. until 5:00p.m. Hours may be reduced on Reduced Schedule service days. Tickets may be purchased aboard the bus, or at MMSA outlets on these days.

ii. When tickets are not being sold from the Adventure Center or loading area, passengers may purchase tickets from shuttle drivers.

iii. ESTA, or its surrogate, will accept all major credit cards and cash for payment of shuttle fares at ticket sales outlets.

iv. Shuttle drivers need only accept cash and need not guarantee correct change for tickets sold aboard the bus.

c. Passengers boarding the shuttle within the Reds Meadow Valley will be charged fares as follows:

i. Passengers initiating shuttle transportation within the Reds Meadow Valley must pay the fare to use the shuttle. The fare is valid for a round-trip, including the trip back into the Valley, if desired.

ii. Passengers who paid the exception vehicle fee to stay in Reds Meadow Valley must pay the fare to use the shuttle to travel out or into the Valley. The fare is valid for a round-trip.

iii. All passengers who travel only to stops within the Reds Meadow Valley will be allowed to ride without paying a fare.

d. ESTA or its surrogate will honor concessionaire or employee passes created by the Forest Service.

i. The Forest Service will limit distribution of concessionaire and employee passes to:

1. Forest Service or National Park Service staff who live or work in Reds Meadow Valley,
 2. Red's Meadow Resort staff who live or work in the Reds Meadow Valley,
 3. Campground concessionaire staff that live or work in the Reds Meadow Valley, and
 4. Outfitters and guides with Special Use Permits to operate in Reds Meadow Valley (pass is not valid for their customers).
- ii. ESTA may document the use of Forest Service season passes to demonstrate the financial impact to revenues.
- e. On-duty Forest Service and National Park Service staff may ride the shuttle at no cost (including work related travel or travel to or from living quarters or duty stations), as room allows.
 - f. All adjustments to the Reds Meadow Shuttle fare structure shall be subject to review and approval by the Forest Service.

2. Shuttle Route

- a. Reds Meadow Shuttles will operate into and out of Reds Meadow Valley between the Mammoth Mountain Adventure Center or The Village depending on schedule, and the Red's Meadow Resort. Each trip will include stops, if requested by passengers or waiting passengers are present, at the following locations: Agnew Meadows, Starkweather Lake, Upper Soda Springs campground, Pumice Flats campground, Minaret Falls campground, Devils Postpile Ranger Station, Sotcher Lake, Reds Meadow campground, Rainbow Falls trailhead and the Red's Meadow Resort.
 - i. Designated shuttles will begin their route at The Village in the Town of Mammoth Lakes.
 - ii. All shuttles will stop at Devils Postpile Ranger Station.
 - iii. Inbound shuttles that are operating in addition to the scheduled times do not need to continue beyond the Devils Postpile Ranger Station and may return direct to the Adventure Center.
 - iv. Outbound shuttles departing from Reds Meadow Resort that are full and have no passengers wishing to disembark at the Devils Postpile Ranger Station need not stop at the Devils Postpile.
- b. The route name, "Reds Meadow-Devils Postpile," "Reds Meadow," or "Devils Postpile" shall be clearly displayed on the shuttle's intergral route sign, when the vehicle is so equipped, or alternatively on a sign placed on the front of the shuttle.
 - i. A sign indicating the route direction shall be clearly displayed on the front of the shuttle.

3. Shuttle Schedule

- a. ESTA or its surrogate will operate the Reds Meadow Shuttle service daily between the anticipated start date, June 11, 2016 and scheduled end date, September 7, 2016.
- b. ESTA will, at a minimum, follow the shuttle schedule attached as Appendix D from June 18, 2016 through August 21, 2016, plus the 3-day Labor Day holiday weekend.

- c. ESTA or its surrogate will provide sufficient shuttle capacity to meet ridership demand to minimize passenger waiting time. Average passenger wait time greater than 30 minutes is considered excessive.
 - d. The Forest Service and ESTA shall jointly develop any revised schedule to appropriately address increased or decreased demand during the operating season, if necessary. ESTA will coordinate any regular adjustments to the shuttle schedule with the Forest Service.
4. Promotional material
- a. Forest Service will design informative /shuttle promotional material that includes, at a minimum:
 - Dates of operations,
 - Hours of operation and daily schedule,
 - Fare structure,
 - Ticket outlet locations and methods of payment,
 - Route map with boarding and alighting locations,
 - Dog ridership information, and
 - US Forest Service and National Park Service logos and any required verbiage.
 - b. ESTA will produce and distribute shuttle promotional material as provided by the Forest Service.
 - c. Forest Service and National Park Service will provide artwork, logos and any required verbiage, in an acceptable format, to ESTA for use on all shuttle schedules, and promotional material.
 - d. Forest Service, National Park Service and ESTA will provide schedule, fare and pertinent rider information (information in Section 1.4.a.) about the Reds Meadow Shuttle service on agency websites.
 - e. Forest Service will distribute promotional media to Inyo National Forest welcome and visitor centers, campgrounds within the Reds Meadow Valley and the Red's Meadow Resort and pack station.
 - f. ESTA will distribute promotional material to Mammoth Mountain Ski Area, The Village, Chambers of Commerce, Yosemite Area Regional Transportation System (YARTS) and key information outlets in the greater Mammoth Mountain area.
 - g. Forest Service and National Park Service will be responsible for any interpretive or promotional material for display or distribution at shuttle stops or on shuttle vehicles.
 - i. ESTA will make available two interior advertising card spaces per bus for Forest Service or National Park Service interpretive or promotional information.
5. Employee education and public interaction
- a. ESTA will ensure all ESTA staff and surrogate staff that interacts with the public is courteous and respectful to the visiting public.
 - b. To ensure ESTA or its surrogate staff provide excellent customer service and are knowledgeable about the area, Forest Service and/or National Park Service will provide personnel, for up to four (4) hours, for Reds Meadow Shuttle staff orientation training on the history, resources and recreational opportunities in the Reds Meadow Valley. This training is intended to empower shuttle drivers, customer service agents and ticket sales

staff with basic, accurate answers to common visitor questions. ESTA, or its surrogate, will make employees who have not previously completed this orientation available for orientation training prior to the inception of shuttle service.

- c. National Park Service will produce and provide copies of a shuttle driver informational tool of the history, resources and recreational opportunities in the Reds Meadow Valley to ESTA for distribution to Reds Meadow Shuttle staff.
- d. Forest Service and/or National Park Service staff may provide on-shuttle interpretative discussion.
- e. ESTA or its surrogate will solicit, document and respond to passenger inquires, comments, and complaints within 72 hours of receipt of such and provide a copy of each with response to the Forest Service Program Manager on a weekly basis. Serious complaints, such as those involving safety or visitor injuries, must be addressed immediately and reported to the Forest Service Program Manager in the most expeditious manner.
- f. ESTA, Forest Service and National Park Service will share copies of all customer comments and complaints received.

6. Coordination

- a. Forest Service and ESTA will develop a program budget that provides the greatest level of service while maximizing the limited resources available for this program.
- b. All sub-contracts or sub-agreements to this agreement shall be coordinated between ESTA and the Forest Service.
- c. Forest Service and ESTA will coordinate shuttle operation beginning and ending dates.
- d. Forest Service will arrange and ESTA will attend a pre-season planning meeting, operating season meetings, as needed, and a post-season wrap-up meeting with appropriate agency staff and key stakeholders.
- e. Forest Service and National Park Service will direct groups requesting transportation into the Reds Meadow Valley to ESTA for proper handling.
- f. Forest Service will monitor shuttle service and coordinate any adjustments to schedule or route with ESTA.
- g. ESTA will provide any details about schedule changes or other pertinent information useful to the public to Mammoth Mountain Ski Area, The Village, Chambers of Commerce, Yosemite Area Regional Transportation System (YARTS) and key information outlets in the greater Mammoth Mountain area.
- h. The Minaret Vista Checkpoint will coordinate with ESTA or its surrogate to allow large RVs, vehicles towing trailers, tour buses and other large vehicles to follow Reds Meadow shuttle buses into and out of the Reds Meadow Valley.
- i. ESTA will provide two-way radio capability to the Minaret Vista Station to allow radio contact with shuttle drivers and supervisor.

7. Reporting

- a. ESTA will submit to the Forest Service Program Manager weekly and monthly operation's reports in the format prescribed in Appendix E. Reports shall be provided in electronic format.

- i. A “week” shall mean Monday through Sunday. Weekly reports are due to the Forest Service Program Manager no later than 5:00pm Wednesday of the following week.
 - ii. A “month” shall mean a calendar month. Monthly reports are due to the Forest Service Program Manager no later than five (5) business days after the last day of the month or end of shuttle service.
 - iii. Weekly and Monthly reports are preliminary and may not include MMSA ticket sales information if this information has not been provided to ESTA by MMSA.
 - b. ESTA will submit a season-end report to the Forest Service Program Manager, within 30 days of cessation of shuttle service. The report shall include, at a minimum, a compilation of weekly and monthly performance statistics detailing the following:
 - Total Fare Revenue
 - Average Passenger Fare
 - Total Expenses
 - Farebox Recovery Ratio
 - Total Vehicle Service Hours
 - Total Vehicle Service Miles
 - Passengers per Vehicle Service Hour
 - Average Number of Buses Utilized per Day
 - Peak Number of Buses Utilized per Day
 - Total Number of Missed Runs
 - Total Number of Maintenance Road Calls
 - General Review of the Season’s Shuttle Service Including Proposed Improvements for Coming Seasons
 - c. A report detailing net program income/loss; operating expense per shuttle trip; operating expense and revenue per passenger; farebox recovery ratio; comparison of actual to budgeted revenues and expenses with an explanation of discrepancies; year-to-year comparisons of key financial and operational statistics; and, a general review of the season’s shuttle service including proposed improvements or amendments to future operations will be provided by December 31, 2016.
 - d. Forest Service Program Manager will coordinate the distribution of reports to the National Park Service.
 - e. Forest Service agrees to maintain passenger entrance and exit count data at the Minaret Vista Station on an ESTA provided computer enabled with a data entry form and to provide information from this spreadsheet to ESTA’s Operation’s Supervisor to assist with planning for service out of the Valley.
8. Emergency Procedures and Safety
 - a. Standee passengers on shuttles shall be allowed. ESTA shall minimize the need for standee passengers through the provision of sufficient service capacity to meet demand. ESTA may permit a limited number of standees on shuttle vehicles, at its discretion, given the passenger is capable of properly restraining themselves and does not present a safety hazard to himself/herself or other passengers, will not block aisle ways with carry-on items and the vehicle meets federal regulations regarding standing passengers. No passenger will be required to stand.

- b. ESTA will provide Forest Service Program Manager a copy of insurance documentation to support coverage as required under the Special Use Permit.
- c. ESTA will provide written verification to the Forest Service Program Manager that a vehicle safety inspection has been performed on each bus and that each bus passed the inspection. The vehicle safety inspection must be performed prior to transporting passengers.
 - i. The minimum requirements for passing the vehicle safety inspection shall be those specified by the California Highway Patrol for transit buses.
 - ii. Prior to operation each day, a vehicle inspection will be completed as required by law. Vehicle Inspection Reports will be carried in all vehicles at all times.
- d. ESTA will provide the Forest Service with copies of all collision and incident reporting protocol and forms that will be used in the event of a safety or security incident.
- e. In the case of any shuttle collisions or incidents requiring medical attention, law enforcement assistance, or any other potentially serious situations, ESTA will follow their established protocol for such incidents.
 - i. Any collision or incident requiring medical attention or involving a fatality shall be reported to the Forest Service Program Manager immediately.
 - ii. Any incident that will block or delay traffic should be communicated as soon as is reasonable to the Minaret Vista Checkpoint.
 - iii. The Forest Service Program Manager shall be notified of all collisions or incidents within 24 hours.
 - iv. ESTA or its surrogate shall provide copies of all collision or incident reports to the Forest Service Program Manager within 72 hours.
- f. Emergency dispatch numbers will be known by all employees.
 - i. Owens Valley Interagency Communications Center (Dispatch): **760-873-2488**
 - ii. Reds Meadow Shuttle supervisor: **760-920-3359**
 - iii. ESTA Mammoth Supervisor **760-914-0354**
 - iv. ESTA Administration: **760-872-1901**
- g. Minor searches within Devils Postpile National Monument will be coordinated through National Park Service. Minor searches include visitors on day hikes where family members or groups have become separated and are reported missing. Contact National Park Service staff at the Devils Postpile Ranger Station or at the **emergency contact line: 760-934-8170**
- h. On major searches, the National Park Service will serve as the initial contact to the Mono County Sheriff's Department; otherwise the reporting party should call 911. All major searches or rescue operations will involve the Mono County Sheriff's Department.
- i. Requests for helicopter rescues will be coordinated through the Mono County Sheriff's Department. Forest Supervisor approval must be obtained prior to all flight for life rescues in any Wilderness area. Approval from the Park Superintendent or their designee must be obtained prior to all flight for life rescues in the Devils Postpile National Monument.
- j. Reds Meadow shuttle drivers will maintain a maximum travel speed within the Reds Meadow Valley to provide a safe and scenic tour for passengers. The maximum speed limit on the entrance/exit grade and Devils Postpile National Monument road is 15 miles per hour and 25 miles per hour on the Valley road. Travel speeds should be slower under adverse driving conditions.

- k. Reds Meadow shuttle drivers will be trained and demonstrate skill on the proper operation of the shuttle vehicle and passing of vehicles in the Reds Meadow Valley operating environment where variable roadway width, steep grades, curvilinear and single lane road sections present operational challenges.
 - l. ESTA or its surrogate will maintain the exterior and interior cleanliness of all vehicles in satisfactory condition at all times.
 - i. Shuttle windows should be cleaned daily.
 - m. Forest Service will maintain and clean the bus stops in the Reds Meadow Valley.
 - n. Idling of shuttles will be in accordance with the California Air Resources Board.
 - o. Forest Service will maintain vegetation along roadways as to minimize damage to shuttle vehicles.
9. Modifications within the scope of the instrument shall be made by mutual consent of the parties, by the issuance of a written modification, signed and dated by all parties, prior to any changes being performed.

Appendix B – Trip Itinerary

The Reds Meadow Shuttle service operates along approximately 15 linear miles from The Village in the Town of Mammoth Lakes to the Reds Meadow Resort at the end of the paved road in the Reds Meadow Valley. The Shuttle makes a limited number of morning stops at the Mammoth Lakes Park & Ride, and The Village. The primary boarding location for the Shuttle is at Adventure Center at Mammoth Mountain Ski Area. The shuttle then makes stops at the Minaret Vista, Agnew Meadows, Starkweather Lake, Upper Soda Springs Campground, Pumice Flat Campground, Minaret Falls Campground, Devils Postpile National Monument, Sotcher Lake, Reds Meadow Campground, Rainbow Falls Trailhead and Reds Meadow Resort. The shuttle provides at least 20-minute frequency service from 10:00am until 4:00pm. During the early morning hours (7:00a.m. to 10:00a.m.) and late afternoon/evening hours (4:00p.m. to 8:30p.m.) the shuttle operates with 45-minute frequency. Early season (prior to June 18, 2016) and late season (after August 21, 2016) service may be on a more limited schedule.

Appendix C – Service Map



Appendix D - Shuttle Schedule

Shuttle to run continuous service 7:15am to 8:30pm daily. At least hourly service from The Village will be provided between 7:15a.m. and 11:00a.m. Additional shuttles will be added to the minimum level of service to meet service demand including advance group reservations.

Typical day - minimum level of service

- 6 Minimum number of vehicles in service per day
- 6 Scheduled pick-up times at The Village (7:15am, 8:00am, 8:45am, 9:30am, then every 30 minutes or less until 4:00pm using the Mammoth Bike Park Shuttle connection)
- 28 Minimum number of round trips per day
- 1,120 Minimum inbound passenger capacity
- 520 Peak period* inbound passenger capacity

Peak Service Holiday weekends - minimum level of service

- 11 Minimum number of vehicles in service per day
- 6 Scheduled pick-up times at The Village
- 47 Minimum number of round trips per day
- 1,880 Minimum inbound passenger capacity
- 1,000 Peak period* inbound passenger capacity

Reduced schedule - minimum level of service (June 11 – 17 and, August 21 - September 7 excluding the Labor Day weekend)

- 4 Number of vehicles in service per day
- 6 Scheduled pick-up times at The Village
- 22 Number of round trips per day
- 960 Total passenger capacity

*Peak period is 9:00am – 1:00pm

2016 Daily Schedule

June 18, 2015 - August 21, 2015 plus Labor Day Weekend
(reduced schedule to be operated before and after these dates)

Pickup/Departure: Every 45 minutes from 7:45 am to 10:00 am,
& 4:00 pm to 7:00pm;
Every 20 minutes from 10:00 am to 4:00 pm

Departure from the Village to Mammoth Mountain Adventure Center

7:15 AM
8:00 AM
8:45 AM
9:30 AM
10:15 AM
11:00 AM

Departure from Mammoth Mountain Adventure Center to Reds Meadow Resort

7:30 AM
8:15 AM
9:00 AM
9:45 AM

from 10:00 am until 4:00 p.m., a bus will depart from the Adventure Center every 20 minutes or less

4:45 PM
5:30 PM
6:15 PM
7:00 PM

Departure from Reds Meadow Resort to Mammoth Mountain Adventure Center

8:15 AM
9:00 AM
9:45 AM
10:30 AM

from 10:30 a.m. until 4:45 p.m., a bus will depart from Reds Meadow Resort every 20 minutes or less

5:30 PM
6:15 PM
7:00 PM
7:45 PM

Use Code: 153
Authorization ID: MLD15042
Contact Name: EASTERN SIERRA TRANSIT AUTHORITY
Expiration Date: 12/31/2015

FS-2700-3f (V.07/2012)
OMB No. 0596-0082

SPECIAL USE APPLICATION & TEMPORARY PERMIT FOR OUTFITTING AND GUIDING

**Authority: Federal Lands Recreation Enhancement Act, 16 U.S.C. 6802(h)
(Ref.: FSH 2709.14, Chapter 50)**

PART II - TEMPORARY SPECIAL USE PERMIT FOR OUTFITTING AND GUIDING

**Authority: Federal Lands Recreation Enhancement Act, 16 U.S.C. 6802(h)
(Ref. FSH 2709.11, section 41.53 and 37.21b)**

EASTERN SIERRA TRANSIT AUTHORITY of PO BOX 1357 BISHOP CA UNITED STATES 93515 (the holder) is hereby authorized to use, subject to the terms of this permit, National Forest System lands described as: Sec. 31, T. 3 S., R. 26 E., MT. DIABLO MERIDIAN as shown in attached Exhibit(s). This authorization covers approximately 1000 acres and/or 1.56 miles.

This temporary use permit is issued for the purpose of authorizing the following outfitting and guiding activities on National Forest System lands:

To provide transportation services to the public visiting the Reds Meadow Valley and associated destinations. 65000 temporary use service days for the delivery of the public from the Main Lodge of Mammoth Mountain Ski Area to destinations in the Reds Meadow Valley.

Wilderness use not authorized. Park only in existing legal parking areas and drive only on existing legal roads. Do not disturb soils, trample vegetation, or harass wildlife. Use must not interfere with visitor enjoyment of public lands. Interact with the public in a courteous manner. All rules and regulations must be followed.

65000 temporary use service days for the delivery of the public from the Main Lodge of Mammoth Mountain Ski Area to destinations in the Reds Meadow Valley.

N/A assigned sites for N/A

N/A grazing use (in head months (HMs))

The following appendices are attached to and made a part of this permit:

APPENDIX A - Operating Plan

APPENDIX B - Trip Itinerary

APPENDIX C - Map of the Authorized Area

APPENDIX D - Site-Specific Conditions

I. GENERAL TERMS AND CONDITIONS

A. AUTHORITY. This permit is issued pursuant to Federal Lands Recreation Enhancement Act, 16 U.S.C. 6802(h), and 36 CFR Part 251, Subpart B, as amended, and is subject to their provisions.

B. AUTHORIZED OFFICER. The authorized officer is the Forest Supervisor or a subordinate officer with delegated authority.

C. TERM. This permit shall expire at midnight on 11/19/2015. The term for this temporary permit shall not exceed 180 days.

D. RENEWAL AND EXTENSION. This permit is not renewable. Upon expiration of the permit all use shall return to the temporary use pool.

E. AMENDMENT. This permit may be amended, provided that the total use authorized not exceed 200 service days or the equivalent in quotas and the term of the permit not exceed 180 days.

F. COMPLIANCE WITH LAWS, REGULATIONS, AND OTHER LEGAL REQUIREMENTS. In exercising the privileges

granted by this permit, the holder shall comply with all present and future federal laws and regulations and all present and future state, county, and municipal laws, regulations, and other legal requirements that fall under the jurisdiction of other governmental entities.

G. NON-EXCLUSIVE USE. The use and occupancy authorized by this permit are not exclusive. The Forest Service reserves the right of access to the permit area, including a continuing right of physical entry to the permit area for inspection, monitoring, or any other purpose consistent with any right or obligation of the United States under any law or regulation. The Forest Service reserves the right to allow others to use the permit area in any way that is not inconsistent with the holder's rights and privileges under this permit, after consultation with all parties involved. Except for any restrictions that the holder and the authorized officer agree are necessary to protect the installation and operation of authorized temporary improvements, the lands and waters covered by this permit shall remain open to the public for all lawful purposes.

H. ASSIGNABILITY. This permit is not assignable or transferable.

II. OPERATIONS

A. OPERATING PLAN. The operating plan submitted in the application corresponding to this permit is incorporated as the operating plan for this permit and is attached as Appendix A. You would be required to notify the Forest Service in writing of any staff changes during your operating season.

B. TRIP ITINERARY. The trip itinerary submitted in the application corresponding to this permit is incorporated as the trip itinerary for this permit and is attached as Appendix B.

C. REQUIRED LICENSES. The holder shall obtain all licenses required for conducting the activities authorized by this permit.

D. CONDITION OF OPERATIONS. The holder shall maintain the permit area to standards of repair, orderliness, neatness, sanitation, and safety acceptable to the authorized officer and consistent with other provisions of this permit.

E. PROHIBITION ON USE OF MECHANIZED TRANSPORT OR MOTORIZED EQUIPMENT IN WILDERNESS AREAS. The holder shall not use mechanized transport or motorized equipment in wilderness areas and shall not use mechanized transport or motorized equipment in proposed or potential wilderness areas without prior written approval from the authorized officer.

F. PROHIBITION ON IMPEDING OR INTERFERING WITH OTHER USES. The holder shall perform the activities authorized by this permit so as not to impede or interfere with administrative or other authorized uses of National Forest System lands.

G. RESTRICTION OF MOTOR VEHICLE USE. The holder shall restrict motor vehicle use to designated roads, trails, and areas, unless specifically provided otherwise in the operating plan.

H. RESOURCE PROTECTION. The holder shall conduct all activities so as to prevent or minimize scarring, erosion, littering, and pollution of National Forest System lands, water pollution, and damage to watersheds. In addition, the holder shall take precautions at all times to prevent wildfire.

I. PERFORMANCE OF SUPPORT SERVICES. As a general rule, the holder shall conduct the day-to-day activities authorized by this permit. Activities that support the use authorized by this permit, such as food or shuttle services, may be conducted by a party other than the holder, but only with prior written approval from the authorized officer. The holder shall continue to be responsible for compliance with all the terms of this permit.

J. NONDISCRIMINATION

1. The holder and its employees shall not discriminate against any person on the basis of race, color, sex (in educational activities), national origin, age, or disability or by curtailing or refusing to furnish accommodations, facilities, services, or use privileges offered to the public generally. In addition, the holder and its employees shall comply with the provisions of Title VI of the Civil Rights Act of 1964 as amended, Section 504 of the Rehabilitation Act of 1973, as amended, Title IX of the Education Amendments Act of 1972, as amended, and the Age Discrimination Act of 1975, as amended.

2. The holder shall include and require compliance with the above nondiscrimination provisions in any third-party agreement made with respect to the operations authorized under this permit.

3. The Forest Service shall furnish signs setting forth this policy of nondiscrimination. These signs shall be conspicuously displayed at the public entrance to the premises and at other exterior or interior locations, as directed by the Forest Service.

4. The Forest Service shall have the right to enforce the foregoing nondiscrimination provisions by suit for specific performance or by any other available remedy under the laws of the United States or the State in which the violation occurs.

K. EQUAL ACCESS TO FEDERAL PROGRAMS. In addition to the above nondiscrimination policy, the holder agrees to ensure that its programs and activities are open to the general public on an equal basis and without regard to any non-merit factor.

L. SANITATION. The operation and maintenance of all sanitation and food service systems and facilities shall comply with applicable standards set by state and local health departments.

M. SIGNS AND TEMPORARY IMPROVEMENTS. Signs posted and temporary improvements installed on National Forest System lands must have prior written approval from the authorized officer.

N. NOXIOUS WEED/EXOTIC PLANT PREVENTION AND CONTROL. The holder shall be responsible for the prevention and control of noxious weeds and/or exotic plants arising from the authorized use. For purposes of this clause, noxious weeds and exotic plants include those species recognized as such by US Fish & Wildlife Service. The holder shall follow prevention and control measures required by Mammoth Ranger District. When determined to be necessary by the authorized officer, the holder shall develop a plan for noxious weed and exotic plant prevention and control. These plans must have prior written approval from the authorized officer and, upon approval, shall be attached to this permit as an appendix.

O. WEED-FREE HAY. The holder shall use only hay, grain, straw, pelletized feed, or mulch certified as noxious weed-free or noxious weed seed-free by a State Department of Agriculture or other authorized county official. The holder shall provide documentation of that certification to the authorized officer.

III. RIGHTS AND LIABILITIES

A. LEGAL EFFECT OF THE PERMIT. This permit, which is revocable and terminable, is not a contract or a lease, but rather a federal license. The benefits and requirements conferred by this authorization are reviewable solely under the procedures set forth in 36 CFR Part 251, Subpart C, and 5 U.S.C. 704. This permit does not constitute a contract for purposes of the Contract Disputes Act, 41 U.S.C. 601. The permit is not real property, does not convey any interest in real property, and may not be used as collateral for a loan.

B. THIRD-PARTY RIGHTS. This permit is subject to all valid outstanding rights. Valid outstanding rights include those derived from mining and mineral leasing laws of the United States. The United States is not liable to the holder for the exercise of any such right.

C. ABSENCE OF THIRD PARTY BENEFICIARY RIGHTS. The parties to this permit do not intend to confer any rights on any third party as a beneficiary under this permit.

D. DAMAGE TO UNITED STATES PROPERTY. The holder has an affirmative duty to protect from damage the land, property, and other interests of the United States. Damage includes but is not limited to fire suppression costs, damage to government-owned improvements covered by this permit, and all costs and damages associated with or resulting from the release or threatened release of a hazardous material occurring during or as a result of activities of the holder or the holder's heirs, assigns, agents, employees, contractors, or lessees on, or related to, the lands, property, and other interests covered by this permit. For purposes of clauses III.D, III.F, and II.H, "hazardous material" shall mean any hazardous substance, pollutant, contaminant, hazardous waste, oil, and/or petroleum product, as those terms are defined under any federal, state, or local law or regulation.

E. INDEMNIFICATION. The holder shall indemnify, defend, and hold harmless the United States for any costs, damages, claims, liabilities, and judgments arising from past, present, and future acts or omissions of the holder in connection with the use and occupancy authorized by this permit. This indemnification and hold harmless provision includes but is not limited to acts and omissions of the holder or the holder's heirs, assigns, agents, employees, contractors, or lessees in connection with the use and occupancy authorized by this permit which result in (1) violations of any laws and regulations which are now or which may in the future become applicable, and including but not limited to those environmental laws listed in clause V.A of this permit; (2) judgments, claims, demands, penalties, or fees assessed against the United States; (3) costs, expenses, and damages incurred by the United States; or (4) the release or threatened release of any solid

waste, hazardous waste, hazardous substance, pollutant, contaminant, oil in any form, or petroleum product into the environment. The authorized officer may prescribe terms that allow the holder to replace, repair, restore, or otherwise undertake necessary curative actions to mitigate damages in addition to or as an alternative to monetary indemnification.

F. INSURANCE. The holder shall furnish proof of insurance, such as a certificate of insurance, to the authorized officer prior to issuance of this permit and each year thereafter that this permit is in effect. The Forest Service reserves the right to review and approve the insurance policy prior to issuance. The holder shall send an authenticated copy of any insurance policy obtained pursuant to this clause to the authorized officer immediately upon issuance of the policy. Any insurance policies obtained by the holder pursuant to this clause shall name the United States as an additional insured, and the additional insured provision shall provide for insurance coverage for the United States as required under this clause. Such policies shall also specify that the insurance company shall give 30 days prior written notice to the authorized officer of cancellation of or any modification to the policies. The certificate of insurance, the authenticated copy of the insurance policy, and written notice of cancellation or modification of insurance policies should be sent to **U.S. Government, c/o Mammoth Ranger District, PO BOX 148 MAMMOTH LAKES, CA 93546**. Minimum amounts of coverage and other insurance requirements are subject to change at the sole discretion of the authorized officer on the anniversary date of this permit.

1. Liability. The holder shall have in force liability insurance covering losses associated with the use and occupancy authorized by this permit arising from personal injury or death and third-party property damage in the minimum amount of:

\$500000 for injury or death to one person per occurrence,
\$1000000 for injury or death to more than one person per occurrence, and
\$100000 for third-party property damage, per occurrence.

2. Depending on the holder's operations, the Forest Service may require the holder to demonstrate the availability of funds to address any release or threatened release of hazardous materials that may occur in connection with the holder's use and occupancy. Any requirements imposed would be established on a case-by-case basis by the authorized officer based on the degree of environmental risk from the holder's operations. The use and storage of normal maintenance supplies in nominal amounts generally would not trigger financial assurance requirements

G. CONTRACTED SERVICES. The holder shall have in force an endorsement covering contracted services and equipment or, alternatively, shall procure a separate insurance policy that covers these services.

IV. LAND USE FEE

A. Permit Fee. The holder shall pay to the USDA, Forest Service, a permit fee for the term of this permit based on the fair market value of the use and occupancy authorized by this permit. The annual permit fee shall include a commercial use fee and, if applicable, an assigned site fee and grazing fee, as enumerated in clauses IV.B, C, and D. The minimum annual permit fee for the authorized use and occupancy shall be **\$105.00**. Estimates of service days, the number of assigned sites, and grazing use shall be determined from the operating plan. Estimated fees shall be calculated on an Estimated Fee Determination Sheet. The holder shall pay the permit fee in advance of the authorized use and occupancy, as provided in clause IV.B. Payments due before commercial operations commence are not refundable. The Forest Service shall adjust and calculate permit fees authorized by this permit to comply with any new permit fee system based on market value that may be adopted by statute, regulation or directive issued by the Chief after issuance of this permit.

1. Commercial Use Fee. The annual permit fee shall be determined in accordance with **option B (3% of gross revenue)**.

(a) Definitions

(1) Adjusted Gross Revenue. Gross revenue and revenue additions less applicable exclusions.

(2) Gross Revenue. The total amount of receipts from the sale of goods or services provided by the holder or third party under the permit.

(3) Revenue Additions. The market value of the following items, which are added to gross revenue:

(A) The value of goods and services that are donated or the value of goods and services that are bartered in exchange for goods and services received that are directly related to the outfitted or guided trip; and

(B) The value of gratuities, which are goods, services, or privileges that are not available to the general public and that are

donated or provided without charge to organizations; individuals; the holder's employees, owners, or officers; or immediate family members of the holder's employees, owners, or officers.

(4) Revenue Exclusions. The following are excluded from gross revenue:

(A) Revenue derived from goods or services sold on private land that are not related to outfitting and guiding operations conducted on National Forest System lands, such as souvenirs, telephone toll charges, and accident insurance sales.

(B) Amounts paid or payable to a State government licensing authority or recreation administering agency from sales of hunting or fishing licenses and recreation fee tickets.

(C) Revenue from the sale of operating equipment, rental equipment, capitalized assets, or other assets used in outfitting and guiding operations, such as horses, tack, watercraft, and rental skis and boots, which are sold periodically and replaced.

2. Assigned Site Fee. A fee shall be charged for the occupancy of National Forest System sites assigned to the holder. Assigned site fees shall not be prorated; the holder shall pay the full annual fee for each assigned site. No refunds or credits will be given for authorized but unused assigned sites.

3. Grazing Fee. A fee shall be charged for grazing livestock used in conjunction with the use and occupancy authorized by this permit. No refunds or credits will be given for authorized but unexercised grazing use.

4. Payment Schedule. The holder shall pay the annual estimated permit fee, including the fee for commercial use, assigned site fee, and grazing fee, in advance of the authorized use, as follows:

(a) Single Payment. The holder shall pay the total annual estimated fee in advance when it is less than \$500.

(b) Two Payments. The holder shall pay half the total annual estimated fee in advance and the remainder by mid-season when the total is equal to or greater than \$500, but less than \$2,500.

(c) Three Payments. The holder shall pay one-third of the total annual estimated fee in advance and the remainder in two equal payments by mid-season when the total is \$2,500 or more.

(d) Final Payment. The Forest Service shall reconcile annually the actual permit fee against permit fee payments made. The holder shall pay any additional fees owed for the past year's operation within 30 days of billing.

5. Documentation of Revenue. The holder shall provide documentation of use and revenue for purposes of permit fee verification.

(a) Actual Use Report. Within 30 days of completion of the holder's approved operating season, the holder shall submit to the authorized officer an actual use report in accordance with the format in Appendix E.

(b) Income Statements. No later than 90 days after the close of the holder's fiscal year, the holder shall submit to the authorized officer a statement of income reporting the results of the holder's annual operations. The statement shall include gross revenue, the value of donated goods and services, the value of gratuities, the value and description of items excluded from gross revenue, and all adjustments, such as taxes deducted, and shall be broken down by permitted activities.

6. Fee Payment Issues

(a) Crediting of Payments. Payments shall be credited on the date received by the deposit facility, except that if a payment is received on a non-workday, the payment shall not be credited until the next workday.

(b) Disputed Fees. Fees are due and payable by the due date. Disputed fees must be paid in full. Adjustments will be made if dictated by settlement terms or an appeal decision.

(c) Late Payments

(A) Interest. Pursuant to 31 U.S.C. 3717 *et seq.*, interest shall be charged on any fee amount not paid within 30 days from the date it became due. The rate of interest assessed shall be the higher of the Prompt Payment Act rate or the rate of the current value of funds to the Treasury (i.e., the Treasury tax and loan account rate), as prescribed and published annually or quarterly by the Secretary of the Treasury in the *Federal Register* and the Treasury Fiscal Requirements

Manual Bulletins. Interest on the principal shall accrue from the date the fee amount is due.

(B) Administrative Costs. If the account becomes delinquent, administrative costs to cover processing and handling the delinquency shall be assessed.

(C) Penalties. A penalty of 6% per annum shall be assessed on the total amount that is more than 90 days delinquent and shall accrue from the same date on which interest charges begin to accrue.

(D) Termination for Nonpayment. This permit shall terminate without the necessity of prior notice and opportunity to comply when any permit fee payment is 90 calendar days from the due date in arrears. The holder shall be responsible for the delinquent fees, as well as any other costs of restoring the site to its original condition, including hazardous waste cleanup.

(d) Administrative Offset and Credit Reporting. Delinquent fees and other charges associated with the permit shall be subject to all rights and remedies afforded the United States pursuant to 31 U.S.C. 3711 *et seq.* and common law. Delinquencies are subject to any or all of the following:

(A) Administrative offset of payments due the holder from the Forest Service.

(B) If in excess of 60 days, referral to the Department of the Treasury for appropriate collection action as provided by 31 U.S.C. 3711(g)(1).

(C) Offset by the Secretary of the Treasury of any amount due the holder, as provided by

31 U.S.C. 3720 *et seq.*

(D) Disclosure to consumer or commercial credit reporting agencies.

7. Accounting Records. The holder shall follow generally accepted accounting principles or another comprehensive basis of accounting, such as the cash, modified cash, or income tax basis of accounting, in recording financial transactions. The minimum acceptable accounting system shall include:

(a) Systematic internal controls and separate recording of gross receipts from each type of business conducted under this permit, separate from any other commercial or personal activity. Receipts shall be recorded daily without reduction and, if possible, deposited into a bank account. Receipt entries shall be supported by documentation such as cash register tapes, sales invoices, reservation records, and cash accounts from other sources.

(b) For permits with fees greater than \$10,000, when requested by the Forest Service, the holder at its own expense shall have its annual accounting records audited by an independent public accountant acceptable to the Forest Service.

B. ACCESS TO ACCOUNTING RECORDS. The holder shall make all of the accounting books and supporting records for the business activities authorized by this permit available for audit by the Forest Service or other federal agencies authorized to review Forest Service activities. The holder shall retain these records and make them available for review for five years after the end of the year they were generated, unless disposition is otherwise authorized by the Forest Service in writing.

V. REVOCATION, SUSPENSION, AND TERMINATION

A. REVOCATION AND SUSPENSION. The authorized officer may revoke or suspend this permit in whole or in part:

1. For noncompliance with federal, state, or local laws and regulations;
2. For noncompliance with the terms of this permit;
3. For failure of the holder to exercise the privileges granted by this permit;
4. With the consent of the holder; or
5. At the discretion of the authorized officer, for specific and compelling reasons in the public interest.

B. NOTICE AND OPPORTUNITY TO TAKE CORRECTIVE ACTION. Prior to revocation or suspension under clause V.A, the authorized officer shall give the holder notice of the grounds for the action to be taken and a reasonable period, not to exceed 30 days, to complete corrective action prescribed by the authorized officer. Failure of the holder to take corrective action shall disqualify the holder from eligibility for another permit for three years.

C. IMMEDIATE SUSPENSION. The authorized officer may immediately suspend this permit in whole or in part when necessary to protect public health or safety or the environment. The suspension decision must be in writing. Within 48 hours of the request of the holder, the superior of the authorized officer shall arrange for an on-the-ground review of the

adverse conditions with the holder. Following this review, the superior shall take prompt action to affirm, modify, or cancel the suspension.

D. APPEALS AND REMEDIES. Any written decisions by the authorized officer relating to administration of this permit, including revocation or suspension decisions, are subject to the administrative appeal regulations at 36 CFR Part 214. Revocation or suspension of this permit shall not give rise to any claim for damages by the holder against the Forest Service.

E. TERMINATION. This permit shall terminate when by its terms a fixed or agreed upon condition, event, or time occurs without any action by the authorized officer. Examples include but are not limited to expiration of the permit by its terms on a specified date. Termination of this permit is not subject to administrative appeal and shall not give rise to any claim for damages by the holder against the Forest Service.

VI. MISCELLANEOUS PROVISIONS

A. ADVERTISING. The holder shall not misrepresent in any way, either orally, in its circulars, brochures, advertising, and other materials, or on its website, signs, or letterheads, any aspect of the use authorized by this permit, including services provided by the holder, the status of this permit, or the area it covers. All of the holder's circulars, brochures, and advertising and its website regarding use of the permit area shall state that the permit area is located on the Inyo National Forest or National Grassland .

B. CURRENT ADDRESSES. The holder and the Forest Service shall keep each other informed of current mailing addresses, including those necessary for payment of fees.

C. SERVICES NOT PROVIDED. This permit does not provide for the furnishing of road or trail maintenance, water, fire protection, search and rescue, or any other service by a government agency, utility, association, or individual.

D. MEMBERS OF CONGRESS. No member of or delegate to Congress or resident commissioner shall benefit from this permit either directly or indirectly, except to the extent the authorized use provides a general benefit to a corporation.

E. SUPERIOR CLAUSES. In the event of any conflict between any of the preceding printed clauses and any subsequent clauses or any provisions in the appendices attached to this permit, the preceding printed clauses shall control.

This permit is accepted subject to all its terms and conditions.

I have read and understand the terms and conditions and agree to abide by them.

By: 
HOLDER: **EASTERN SIERRA TRANSIT AUTHORITY**

Title: EXECUTIVE DIRECTOR

Date: MAY 15, 2015

U.S. DEPARTMENT OF AGRICULTURE
Forest Service

Authorization is granted:
By: _____

Name: **JON C REGELBRUGGE**

Title: **DISTRICT RANGER**

Date: _____

HOLDER MUST HAVE THIS PERMIT (OR A LEGIBLE COPY) IN POSSESSION DURING THE AUTHORIZED ACTIVITY

According to the Paperwork Reduction Act of 1995, an agency may not conduct or sponsor, and a person is not required to respond to a collection of information unless it displays a valid OMB control number. The valid OMB control number for this information collection is 0596-0082. The time required to complete this information collection is estimated to average 4 hours per response, including the time for reviewing instructions, searching existing data sources, gathering and maintaining the data needed, and completing and reviewing the collection of information.
The U.S. Department of Agriculture (USDA) prohibits discrimination in all its programs and activities on the basis of race, color, national origin, age, disability, and where applicable, sex, marital status, familial status, parental status, religion, sexual orientation, genetic information, political beliefs, reprisal, or because all or part of an individual's income is derived from any public assistance. (Not all prohibited bases apply to all programs.) Persons with disabilities who require alternative means for communication of program information (Braille, large print, audiotape, etc.) should contact USDA's TARGET Center at 202-720-2600 (voice and TDD).
To file a complaint of discrimination, write USDA, Director, Office of Civil Rights, 1400 Independence Avenue, SW, Washington, DC 20250-9410 or call toll free (866) 632-9992 (voice), TDD users can contact USDA through local relay or the Federal relay at (800) 877-8339 (TDD) or (866) 377-8642 (relay voice). USDA is an equal opportunity provider and employer.
The Privacy Act of 1974 (5 U.S.C. 552a) and the Freedom of Information Act (5 U.S.C. 552) govern the confidentiality to be provided for information received by the Forest Service.

adverse conditions with the holder. Following this review, the superior shall take prompt action to affirm, modify, or cancel the suspension.

D. APPEALS AND REMEDIES. Any written decisions by the authorized officer relating to administration of this permit, including revocation or suspension decisions, are subject to the administrative appeal regulations at 36 CFR Part 214. Revocation or suspension of this permit shall not give rise to any claim for damages by the holder against the Forest Service.

E. TERMINATION. This permit shall terminate when by its terms a fixed or agreed upon condition, event, or time occurs without any action by the authorized officer. Examples include but are not limited to expiration of the permit by its terms on a specified date. Termination of this permit is not subject to administrative appeal and shall not give rise to any claim for damages by the holder against the Forest Service.

VI. MISCELLANEOUS PROVISIONS

A. ADVERTISING. The holder shall not misrepresent in any way, either orally, in its circulars, brochures, advertising, and other materials, or on its website, signs, or letterheads, any aspect of the use authorized by this permit, including services provided by the holder, the status of this permit, or the area it covers. All of the holder's circulars, brochures, and advertising and its website regarding use of the permit area shall state that the permit area is located on the Inyo National Forest or National Grassland.

B. CURRENT ADDRESSES. The holder and the Forest Service shall keep each other informed of current mailing addresses, including those necessary for payment of fees.

C. SERVICES NOT PROVIDED. This permit does not provide for the furnishing of road or trail maintenance, water, fire protection, search and rescue, or any other service by a government agency, utility, association, or individual.

D. MEMBERS OF CONGRESS. No member of or delegate to Congress or resident commissioner shall benefit from this permit either directly or indirectly, except to the extent the authorized use provides a general benefit to a corporation.

E. SUPERIOR CLAUSES. In the event of any conflict between any of the preceding printed clauses and any subsequent clauses or any provisions in the appendices attached to this permit, the preceding printed clauses shall control.

This permit is accepted subject to all its terms and conditions.

I have read and understand the terms and conditions and agree to abide by them.

By: *John Helm*
HOLDER, EASTERN SIERRA TRANSIT AUTHORITY

Title: EXECUTIVE DIRECTOR

Date: MAY 15, 2015

U.S. DEPARTMENT OF AGRICULTURE
Forest Service

Authorization is granted:

By: *Jon C. Regelbrugge*

Name: JON C REGELBRUGGE

Title: DISTRICT RANGER

Date: 5/20/15

HOLDER MUST HAVE THIS PERMIT (OR A LEGIBLE COPY) IN POSSESSION DURING THE AUTHORIZED ACTIVITY

According to the Paperwork Reduction Act of 1995, an agency may not conduct or sponsor, and a person is not required to respond to a collection of information unless it displays a valid OMB control number. The valid OMB control number for this information collection is 0596-0062. The time required to complete this information collection is estimated to average 4 hours per response, including the time for reviewing instructions, searching existing data sources, gathering and maintaining the data needed, and completing and reviewing the collection of information.

The U.S. Department of Agriculture (USDA) prohibits discrimination in all its programs and activities on the basis of race, color, national origin, age, disability, and where applicable, sex, marital status, familial status, parental status, religion, sexual orientation, genetic information, political beliefs, reprisal, or because all or part of an individual's income is derived from any public assistance. (Not all prohibited bases apply to all programs.) Persons with disabilities who require alternative means for communication of program information (Braille, large print, audiotape, etc.) should contact USDA's TARGET Center at 202-720-2600 (voice and TDD).

To file a complaint of discrimination, write USDA, Director, Office of Civil Rights, 1400 Independence Avenue, SW Washington, DC 20250-9410 or call toll free (866) 632-9992 (voice). TDD users can contact USDA through local relay or the Federal relay at (800) 877-8339 (TDD) or (866) 377-8642 (relay voice). USDA is an equal opportunity provider and employer. The Privacy Act of 1974 (5 U.S.C. 552a) and the Freedom of Information Act (5 U.S.C. 552) govern the confidentiality to be provided for information received by the Forest Service.

2015 Operating Plan

Eastern Sierra Transit Authority (ESTA)
PO Box 1357, Mammoth Lakes, CA, 93546
760-872-1901

John Helm

Prepared By – John Helm (ESTA)

2/27/2015

Date



Reviewed By – Permit Administrator

2/27/15

Date



Approved By – District Ranger
JON C REGELBRUGGE

2/27/15

Date

Appendix A - 2015 ANNUAL OPERATING PLAN
Eastern Sierra Transit Authority, Reds Meadow Shuttle Service

I. GENERAL OVERVIEW OF OPERATIONS

This operating plan outlines the expectations and requirements of Eastern Sierra Transit Authority (ESTA), the Inyo National Forest and the Devils Postpile National Monument National Park Service for the Reds Meadow Shuttle service.

1. Shuttle Fees

a. Shuttle fares are as follows:

Adult (over 15 years old) daily pass:	\$7.00
Child (aged 3 – 15 years old) daily pass:	\$4.00
Infant (< 2 years old):	no charge
Adult 3-day pass (good for 3 out of 5 days):	\$14.00
Child 3-day pass (good for 3 out of 5 days):	\$8.00
Adult season pass (good for all of 2011):	\$35.00
Child season pass (good for all of 2011):	\$20.00
Dog:	no charge

b. ESTA or its surrogate will collect passenger fares during all hours of shuttle operations and provide sufficient staffing and/or ticket vending machines to minimize passenger wait time for ticket purchase.

i. ESTA, or its surrogate, will sell shuttle tickets from a dedicated ticket sales window at the Adventure Center or bus loading area from 8:00a.m. until 5:00p.m. Hours may be reduced on Reduced Schedule service days. Tickets may be purchased aboard the bus, or at MMSA outlets on these days.

ii. When tickets are not being sold from the Adventure Center or loading area, passengers may purchase tickets from shuttle drivers.

iii. ESTA, or its surrogate, will accept all major credit cards and cash for payment of shuttle fares at ticket sales outlets.

iv. Shuttle drivers need only accept cash and need not guarantee correct change for tickets sold aboard the bus.

c. Passengers boarding the shuttle within the Reds Meadow Valley will be charged fares as follows:

i. Passengers initiating shuttle transportation within the Reds Meadow Valley must pay the fare to use the shuttle. The fare is valid for a round-trip, including the trip back into the Valley, if desired.

ii. Passengers who paid the exception vehicle fee to stay in Reds Meadow Valley must pay the fare to use the shuttle to travel out or into the Valley. The fare is valid for a round-trip.

iii. All passengers who travel only to stops within the Reds Meadow Valley will be allowed to ride without paying a fare.

d. ESTA or its surrogate will honor concessionaire or employee passes created by the Forest Service.

i. The Forest Service will limit distribution of concessionaire and employee passes to:

1. Forest Service or National Park Service staff who live or work in Reds Meadow Valley,
2. Red's Meadow Resort staff who live or work in the Reds Meadow Valley,

- 3. Campground concessionaire staff that live or work in the Reds Meadow Valley, and
 - 4. Outfitters and guides with Special Use Permits to operate in Reds Meadow Valley (pass is not valid for their customers).
 - ii. ESTA may document the use of Forest Service season passes to demonstrate the financial impact to revenues.
 - e. On-duty Forest Service and National Park Service staff may ride the shuttle at no cost (including work related travel or travel to or from living quarters or duty stations), as room allows.
 - f. All adjustments to the Reds Meadow Shuttle fare structure shall be subject to review and approval by the Forest Service.
2. Shuttle Route
- a. Reds Meadow Shuttles will operate into and out of Reds Meadow Valley between the Mammoth Mountain Adventure Center or The Village depending on schedule, and the Red's Meadow Resort. Each trip will include stops, if requested by passengers or waiting passengers are present, at the following locations: Agnew Meadows, Starkweather Lake, Upper Soda Springs campground, Pumice Flats campground, Minaret Falls campground, Devils Postpile Ranger Station, Sotcher Lake, Reds Meadow campground, Rainbow Falls trailhead and the Red's Meadow Resort.
 - i. Designated shuttles will begin their route at The Village in the Town of Mammoth Lakes.
 - ii. All shuttles will stop at Devils Postpile Ranger Station.
 - iii. Inbound shuttles that are operating in addition to the scheduled times do not need to continue beyond the Devils Postpile Ranger Station and may return direct to the Adventure Center.
 - iv. Outbound shuttles departing from Reds Meadow Resort that are full and have no passengers wishing to disembark at the Devils Postpile Ranger Station need not stop at the Devils Postpile.
 - b. The route name, "Reds Meadow-Devils Postpile," "Reds Meadow," or "Devils Postpile" shall be clearly displayed on the shuttle's intergral route sign, when the vehicle is so equipped, or alternatively on a sign placed on the front of the shuttle.
 - i. A sign indicating the route direction shall be clearly displayed on the front of the shuttle.
3. Shuttle Schedule
- a. ESTA or its surrogate will operate the Reds Meadow Shuttle service daily between the anticipated start date, June 13, 2015 and scheduled end date, September 9, 2015. In addition, it is expected that the shuttle service will operate Saturday, Sunday, and Monday of Memorial Day weekend, 2015.
 - b. ESTA will, at a minimum, follow the shuttle schedule attached as Appendix D from June 20, 2015 through August 23, 2015, plus the 3-day Memorial and Labor Day holiday weekends.
 - c. ESTA or its surrogate will provide sufficient shuttle capacity to meet ridership demand to minimize passenger waiting time. Average passenger wait time greater than 30 minutes is considered excessive.
 - d. The Forest Service and ESTA shall jointly develop any revised schedule to appropriately address increased or decreased demand during the operating season, if necessary. ESTA will coordinate any regular adjustments to the shuttle schedule with the Forest Service.

4. Promotional material
 - a. Forest Service will design informative /shuttle promotional material that includes, at a minimum:
 - Dates of operations,
 - Hours of operation and daily schedule,
 - Fare structure,
 - Ticket outlet locations and methods of payment,
 - Route map with boarding and alighting locations,
 - Dog ridership information, and
 - US Forest Service and National Park Service logos and any required verbiage.
 - b. ESTA will produce and distribute shuttle promotional material as provided by the Forest Service.
 - c. Forest Service and National Park Service will provide artwork, logos and any required verbiage, in an acceptable format, to ESTA for use on all shuttle schedules, and promotional material.
 - d. Forest Service, National Park Service and ESTA will provide schedule, fare and pertinent rider information (information in Section 1.4.a.) about the Reds Meadow Shuttle service on agency websites.
 - e. Forest Service will distribute promotional media to Inyo National Forest welcome and visitor centers, campgrounds within the Reds Meadow Valley and the Red's Meadow Resort and pack station.
 - f. ESTA will distribute promotional material to Mammoth Mountain Ski Area, The Village, Chambers of Commerce, Yosemite Area Regional Transportation System (YARTS) and key information outlets in the greater Mammoth Mountain area.
 - g. Forest Service and National Park Service will be responsible for any interpretive or promotional material for display or distribution at shuttle stops or on shuttle vehicles.
 - i. ESTA will make available two interior advertising card spaces per bus for Forest Service or National Park Service interpretive or promotional information.
5. Employee education and public interaction
 - a. ESTA will ensure all ESTA staff and surrogate staff that interacts with the public is courteous and respectful to the visiting public.
 - b. To ensure ESTA or its surrogate staff provide excellent customer service and are knowledgeable about the area, Forest Service and/or National Park Service will provide personnel, for up to four (4) hours, for Reds Meadow Shuttle staff orientation training on the history, resources and recreational opportunities in the Reds Meadow Valley. This training is intended to empower shuttle drivers, customer service agents and ticket sales staff with basic, accurate answers to common visitor questions. ESTA, or its surrogate, will make employees who have not previously completed this orientation available for orientation training prior to the inception of shuttle service.
 - c. National Park Service will produce and provide copies of a shuttle driver informational tool of the history, resources and recreational opportunities in the Reds Meadow Valley to ESTA for distribution to Reds Meadow Shuttle staff.
 - d. Forest Service and/or National Park Service staff may provide on-shuttle interpretative discussion.
 - e. ESTA or its surrogate will solicit, document and respond to passenger inquiries, comments, and complaints within 72 hours of receipt of such and provide a copy of

each with response to the Forest Service Program Manager on a weekly basis. Serious complaints, such as those involving safety or visitor injuries, must be addressed immediately and reported to the Forest Service Program Manager in the most expeditious manner.

- f. ESTA, Forest Service and National Park Service will share copies of all customer comments and complaints received.

6. Coordination

- a. Forest Service and ESTA will develop a program budget that provides the greatest level of service while maximizing the limited resources available for this program.
- b. All sub-contracts or sub-agreements to this agreement shall be coordinated between ESTA and the Forest Service.
- c. Forest Service and ESTA will coordinate shuttle operation beginning and ending dates.
- d. Forest Service will arrange and ESTA will attend a pre-season planning meeting, operating season meetings, as needed, and a post-season wrap-up meeting with appropriate agency staff and key stakeholders.
- e. Forest Service and National Park Service will direct groups requesting transportation into the Reds Meadow Valley to ESTA for proper handling.
- f. Forest Service will monitor shuttle service and coordinate any adjustments to schedule or route with ESTA.
- g. ESTA will provide any details about schedule changes or other pertinent information useful to the public to Mammoth Mountain Ski Area, The Village, Chambers of Commerce, Yosemite Area Regional Transportation System (YARTS) and key information outlets in the greater Mammoth Mountain area.
- h. The Minaret Vista Checkpoint will coordinate with ESTA or its surrogate to allow large RVs, vehicles towing trailers, tour buses and other large vehicles to follow Reds Meadow shuttle buses into and out of the Reds Meadow Valley.
- i. ESTA will provide two-way radio capability to the Minaret Vista Station to allow radio contact with shuttle drivers and supervisor.

7. Reporting

- a. ESTA will submit to the Forest Service Program Manager weekly and monthly operation's reports in the format prescribed in Appendix E. Reports shall be provided in electronic format.
 - i. A "week" shall mean Monday through Sunday. Weekly reports are due to the Forest Service Program Manager no later than 5:00pm Wednesday of the following week.
 - ii. A "month" shall mean a calendar month. Monthly reports are due to the Forest Service Program Manager no later than five (5) business days after the last day of the month or end of shuttle service.
 - iii. Weekly and Monthly reports may not include MMSA ticket sales information if this information has not been provided to ESTA by MMSA.
- b. ESTA will submit a season-end report to the Forest Service Program Manager, within 30 days of cessation of shuttle service. The report shall include, at a minimum, a compilation of weekly and monthly performance statistics detailing the following:
 - Total Fare Revenue
 - Average Passenger Fare
 - Total Expenses
 - Farebox Recovery Ratio

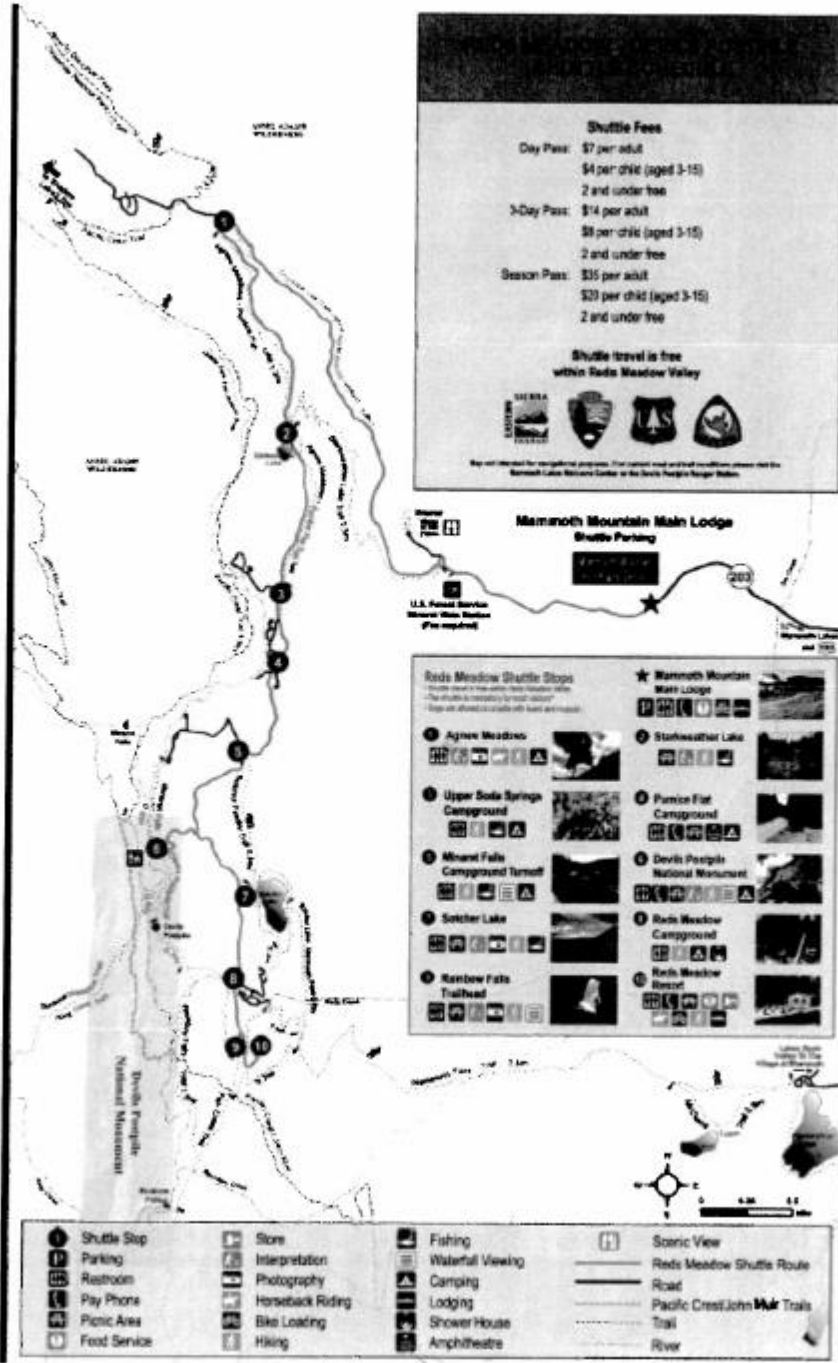
- Total Vehicle Service Hours
 - Total Vehicle Service Miles
 - Passengers per Vehicle Service Hour
 - Average Number of Buses Utilized per Day
 - Peak Number of Buses Utilized per Day
 - Total Number of Missed Runs
 - Total Number of Maintenance Road Calls
 - General Review of the Season's Shuttle Service Including Proposed Improvements for Coming Seasons
- c. A report detailing net program income/loss; operating expense per shuttle trip; operating expense and revenue per passenger; farebox recovery ratio; comparison of actual to budgeted revenues and expenses with an explanation of discrepancies; year-to-year comparisons of key financial and operational statistics; and, a general review of the season's shuttle service including proposed improvements or amendments to future operations will be provided by December 31, 2015.
- d. Forest Service Program Manager will coordinate the distribution of reports to the National Park Service.
- e. Forest Service agrees to maintain passenger entrance and exit count data at the Minaret Vista Station on an ESTA provided computer enabled with a data entry form and to provide information from this spreadsheet to ESTA's Operation's Supervisor to assist with planning for service out of the Valley.
8. Emergency Procedures and Safety
- a. Standee passengers on shuttles shall be allowed. ESTA shall minimize the need for standee passengers through the provision of sufficient service capacity to meet demand. ESTA may permit a limited number of standees on shuttle vehicles, at its discretion, given the passenger is capable of properly restraining themselves and does not present a safety hazard to himself/herself or other passengers, will not block aisle ways with carry-on items and the vehicle meets federal regulations regarding standing passengers. No passenger will be required to stand.
- b. ESTA will provide Forest Service Program Manager a copy of insurance documentation to support coverage as required under the Special Use Permit.
- c. ESTA will provide written verification to the Forest Service Program Manager that a vehicle safety inspection has been performed on each bus and that each bus passed the inspection. The vehicle safety inspection must be performed prior to transporting passengers.
- i. The minimum requirements for passing the vehicle safety inspection shall be those specified by the California Highway Patrol for transit buses.
 - ii. Prior to operation each day, a vehicle inspection will be completed as required by law. Vehicle Inspection Reports will be carried in all vehicles at all times.
- d. ESTA will provide the Forest Service with copies of all collision and incident reporting protocol and forms that will be used in the event of a safety or security incident.
- e. In the case of any shuttle collisions or incidents requiring medical attention, law enforcement assistance, or any other potentially serious situations, ESTA will follow their established protocol for such incidents.
- i. Any collision or incident requiring medical attention or involving a fatality shall be reported to the Forest Service Program Manager immediately.

- ii. Any incident that will block or delay traffic should be communicated as soon as is reasonable to the Minaret Vista Checkpoint.
 - iii. The Forest Service Program Manager shall be notified of all collisions or incidents within 24 hours.
 - iv. ESTA or its surrogate shall provide copies of all collision or incident reports to the Forest Service Program Manager within 72 hours.
 - f. Emergency dispatch numbers will be known by all employees.
 - i. Owens Valley Interagency Communications Center (Dispatch): **760-873-2488**
 - ii. Reds Meadow Shuttle supervisor: **760-920-3359**
 - iii. ESTA Mammoth Supervisor **760-914-0354**
 - iv. ESTA Administration: **760-872-1901**
 - g. Minor searches within Devils Postpile National Monument will be coordinated through National Park Service. Minor searches include visitors on day hikes where family members or groups have become separated and are reported missing. Contact National Park Service staff at the Devils Postpile Ranger Station or at the **emergency contact line: 760-934-8170**
 - h. On major searches, the National Park Service will serve as the initial contact to the Mono County Sheriff's Department; otherwise the reporting party should call 911. All major searches or rescue operations will involve the Mono County Sheriff's Department.
 - i. Requests for helicopter rescues will be coordinated through the Mono County Sheriff's Department. Forest Supervisor approval must be obtained prior to all flight for life rescues in any Wilderness area. Approval from the Park Superintendent or their designee must be obtained prior to all flight for life rescues in the Devils Postpile National Monument.
 - j. Reds Meadow shuttle drivers will maintain a maximum travel speed within the Reds Meadow Valley to provide a safe and scenic tour for passengers. The maximum speed limit on the entrance/exit grade and Devils Postpile National Monument road is 15 miles per hour and 25 miles per hour on the Valley road. Travel speeds should be slower under adverse driving conditions.
 - k. Reds Meadow shuttle drivers will be trained and demonstrate skill on the proper operation of the shuttle vehicle and passing of vehicles in the Reds Meadow Valley operating environment where variable roadway width, steep grades, curvilinear and single lane road sections present operational challenges.
 - l. ESTA or its surrogate will maintain the exterior and interior cleanliness of all vehicles in satisfactory condition at all times.
 - i. Shuttle windows should be cleaned daily.
 - m. Forest Service will maintain and clean the bus stops in the Reds Meadow Valley.
 - n. Idling of shuttles will be in accordance with the California Air Resources Board.
 - o. Forest Service will maintain vegetation along roadways as to minimize damage to shuttle vehicles.
9. Modifications within the scope of the instrument shall be made by mutual consent of the parties, by the issuance of a written modification, signed and dated by all parties, prior to any changes being performed.

Appendix B – Trip Itinerary

The Reds Meadow Shuttle service operates along approximately 15 linear miles from The Village in the Town of Mammoth Lakes to the Reds Meadow Resort at the end of the paved road in the Reds Meadow Valley. The Shuttle makes a limited number of morning stops at the Mammoth Lakes Park & Ride, and The Village. The primary boarding location for the Shuttle is at Adventure Center at Mammoth Mountain Ski Area. The shuttle then makes stops at the Minaret Vista, Agnew Meadows, Starkweather Lake, Upper Soda Springs Campground, Pumice Flat Campground, Minaret Falls Campground, Devils Postpile National Monument, Sotcher Lake, Reds Meadow Campground, Rainbow Falls Trailhead and Reds Meadow Resort. The shuttle provides at least 20-minute frequency service from 10:00am until 4:00pm. During the early morning hours (7:00a.m. to 10:00a.m.) and late afternoon/evening hours (4:00p.m. to 8:30p.m.) the shuttle operates with 45-minute frequency. Early season (prior to June 20, 2015) and late season (after August 23, 2015) service may be on a more limited schedule.

Appendix C – Service Map



Appendix D - Shuttle Schedule

Shuttle to run continuous service 7:15am to 8:30pm daily. At least hourly service from The Village will be provided between 7:15a.m. and 11:00a.m. Additional shuttles will be added to the minimum level of service to meet service demand including advance group reservations.

Typical day - minimum level of service

- 6 Minimum number of vehicles in service per day
- 6 Scheduled pick-up times at The Village (7:15am, 8:00am, 8:45am, 9:30am, then every 30 minutes or less until 4:00pm using the Mammoth Bike Park Shuttle connection)
- 28 Minimum number of round trips per day
- 1,120 Minimum inbound passenger capacity
- 520 Peak period* inbound passenger capacity

Peak Service Holiday weekends - minimum level of service

- 11 Minimum number of vehicles in service per day
- 6 Scheduled pick-up times at The Village
- 47 Minimum number of round trips per day
- 1,880 Minimum inbound passenger capacity
- 1,000 Peak period* inbound passenger capacity

Reduced schedule - minimum level of service (June 13 – 19 and, August 23 - September 9 excluding the Labor Day weekend)

- 4 Number of vehicles in service per day
- 6 Scheduled pick-up times at The Village
- 22 Number of round trips per day
- 960 Total passenger capacity

*Peak period is 9:00am – 1:00pm

2015 Daily Schedule

June 20, 2015 - August 23, 2015
(reduced schedule to be operated before and after these dates)

Pickup/Departure: Every 45 minutes from 7:45 am to 10:00 am,
& 4:00 pm to 7:00pm;
Every 20 minutes from 10:00 am to 4:00 pm

Departure from the Village to Mammoth Mountain Adventure Center
7:15 AM
8:00 AM
8:45 AM
9:30 AM
10:15 AM
11:00 AM
Departure from Mammoth Mountain Adventure Center to Reds Meadow Resort
7:30 AM
8:15 AM
9:00 AM
9:45 AM
from 10:00 am until 4:00 p.m., a bus will depart from the Adventure Center every 20 minutes or less
4:45 PM
6:30 PM
6:15 PM
7:00 PM
Departure from Reds Meadow Resort to Mammoth Mountain Adventure Center
8:15 AM
9:00 AM
9:45 AM
10:30 AM
from 10:30 a.m. until 4:45 p.m., a bus will depart from Reds Meadow Resort every 20 minutes or less
5:30 PM
6:15 PM
7:00 PM
7:45 PM

Appendix E
Weekly and monthly report templates.

Reds Meadow/Dewis Postpile Shuttle 2015 Operating Season - Weekly Report Monday - Sunday WEEK #00							
	Mon	Tue	Wed	Thur	Fri	Sat	Sun
	6/29	6/30	7/1	7/2	7/3	7/4	7/5
Number of shuttle vehicles utilized							
Inbound							
Number of shuttle trips - total							
Number of shuttle trips - peak period ²							
Shuttle ridership - actual ¹							
Shuttle ridership - peak period ²							
Outbound							
Shuttle ridership - actual ²							
Service Hours							
Service Miles							
Missed Runs							
Maintenance Roadcalls							
Incidents/Accidents							
Complaints/Comments							

Service	
Load factor - peak	#DIV/0!
Load factor - average	#DIV/0!
Shuttles per day - average	#DIV/0!
Shuttle trips per day - average	0.00
Service hours	0
Service miles	0
Missed runs	0
Maintenance Roadcalls	0
Incidents/Accidents	0
Complaints/Comments ³	0

Revenue	
Fare Revenue (\$)	\$0
Fare Revenue Y-T-D(\$)	\$0

Other	
Dogs	
Bicycles	

Comments:

Reds Meadow/Dewis Postpile Shuttle 2015 Operating Season - Monthly Report JUNE														
Date	# Shuttle Trips	Inbound Passenger Count ¹	Total Passenger Trips ²	Village Boardings	Ticket Sales - Ticket Office				On Board Ticket Sales			Total Fare Revenue		
					Adult	Child	<2	3 of 5 (Days)	Season Pass	Ticket Office \$5	Adult		Child	Onboard fare
6/1/2015														\$0
6/14/2015														\$0
6/15/2015														\$0
6/16/2015														\$0
6/17/2015														\$0
6/18/2015														\$0
6/19/2015														\$0
6/20/2015														\$0
6/21/2015														\$0
6/22/2015														\$0
6/23/2015														\$0
6/24/2015														\$0
6/25/2015														\$0
6/26/2015														\$0
6/27/2015														\$0
6/28/2015														\$0
6/29/2015														\$0
6/30/2015														\$0
Total	0	0	0	0	0	0	0	0	0	0	0	0	0	\$0

STAFF REPORT

Subject: Financial Report – FY 2015/16

Initiated by: John Helm, Executive Director

ESTA financial reports for FY 2015/16 were prepared on May 18, 2016, which is 88% through the fiscal year. Total revenues are at 79% of the annual budget amount, and total expenses are at 72% of the year’s budget. Receipt of revenues is continuing generally according to anticipated timing. MMSA revenues will end the year lower than budget due to service reductions implemented in May. Although both revenues and expenses normally lag the calendar as has been the case the entire year, reduction in the major expense category of fuel continues to run significantly below budget as a result of lower than forecast fuel prices. Charges from the Town of Mammoth Lakes for fuel and maintenance have been received and posted only through February.

Overtime expense has exceeded the annual budget amount due to turnover and unexpected driver absences, although, total salary expense for the year is projected to be below budget. A report of the projected year-end financial picture is presented as the next agenda item 10.b.

The table below summarizes the expenses by major expense category.

ESTA Operating Expenses			
Percent of the fiscal year			88.2%
Category	Budget	Actual as of 05.18.16	% of Budget
Total Salaries	1,728,564	1,439,415	83.3%
Total Benefits	750,636	579,966	77.3%
Total Insurance	302,890	280,356	92.6%
Total Maintenance	585,160	341,585	58.4%
Facilities	229,570	167,260	72.9%
Total Services	240,830	204,332	84.8%
Fuel	707,140	271,765	38.4%
Other	163,578	84,256	51.5%
	4,708,368	3,368,935	71.6%

The roll-up, the budget unit summary, and the fund balance report for FY 2015/16 as prepared on May 18, 2016, are included on the following pages.

COUNTY OF INYO
Budget to Actuals with Encumbrances by Key/Obj

Ledger: GL

As of 5/18/2016

Object	Description	Budget	Actual	Encumbrance	Balance	%
Key: 153299 - EASTERN SIERRA TRANSIT						
OPERATING						
Revenue						
4061	LOCAL TRANSPORTATION TAX	1,232,955.00	961,182.72	0.00	271,772.28	77.95
4065	STATE TRANSIT ASST	322,876.00	68,846.00	0.00	254,030.00	21.32
4301	INTEREST FROM TREASURY	2,600.00	9,054.80	0.00	(6,454.80)	348.26
4499	STATE OTHER	111,620.00	66,076.20	0.00	45,543.80	59.19
4555	FEDERAL GRANTS	594,951.00	339,541.13	0.00	255,409.87	57.07
4599	OTHER AGENCIES	849,814.00	743,199.91	0.00	106,614.09	87.45
4819	SERVICES & FEES	1,827,906.00	1,697,026.49	0.00	130,879.51	92.83
4959	MISCELLANEOUS REVENUE	5,000.00	4,582.16	0.00	417.84	91.64
	Revenue Total:	<u>4,947,722.00</u>	<u>3,889,509.41</u>	<u>0.00</u>	<u>1,058,212.59</u>	<u>78.61</u>
Expenditure						
5001	SALARIED EMPLOYEES	1,163,600.00	956,866.78	0.00	206,733.22	82.23
5003	OVERTIME	32,230.00	36,275.62	0.00	(4,045.62)	112.55
5005	HOLIDAY OVERTIME	101,606.00	87,563.21	0.00	14,042.79	86.17
5012	PART TIME EMPLOYEES	431,128.00	358,709.11	0.00	72,418.89	83.20
5021	RETIREMENT & SOCIAL SECURITY	40,306.00	34,300.31	0.00	6,005.69	85.09
5022	PERS RETIREMENT	229,100.00	180,918.49	0.00	48,181.51	78.96
5031	MEDICAL INSURANCE	298,940.00	245,513.94	0.00	53,426.06	82.12
5043	OTHER BENEFITS	32,290.00	21,111.42	0.00	11,178.58	65.38
5045	COMPENSATED ABSENCE EXPENSE	143,500.00	95,940.62	0.00	47,559.38	66.85
5047	EMPLOYEE INCENTIVES	6,500.00	2,181.24	0.00	4,318.76	33.55
5111	CLOTHING	8,400.00	3,285.98	0.00	5,114.02	39.11
5152	WORKERS COMPENSATION	90,890.00	90,353.00	0.00	537.00	99.40
5154	UNEMPLOYMENT INSURANCE	45,000.00	22,302.00	0.00	22,698.00	49.56
5158	INSURANCE PREMIUM	167,000.00	167,701.00	0.00	(701.00)	100.41
5171	MAINTENANCE OF EQUIPMENT	544,860.00	326,509.01	21,600.00	196,750.99	63.88
5173	MAINTENANCE OF	22,300.00	12,807.23	0.00	9,492.77	57.43
5191	MAINTENANCE OF STRUCTURES	18,000.00	2,269.00	0.00	15,731.00	12.60
5211	MEMBERSHIPS	2,142.00	610.00	0.00	1,532.00	28.47
5232	OFFICE & OTHER EQUIP < \$5,000	11,100.00	4,764.58	0.00	6,335.42	42.92
5238	OFFICE SUPPLIES	9,186.00	6,288.40	0.00	2,897.60	68.45
5253	ACCOUNTING & AUDITING SERVICE	40,550.00	36,100.00	0.00	4,450.00	89.02
5254	AUDITING SERVICE	8,360.00	0.00	0.00	8,360.00	0.00
5260	HEALTH - EMPLOYEE PHYSICALS	6,400.00	4,688.76	0.00	1,711.24	73.26
5263	ADVERTISING	36,600.00	29,629.13	0.00	6,970.87	80.95
5265	PROFESSIONAL & SPECIAL SERVICE	148,920.00	133,913.82	6,900.02	8,106.16	94.55
5291	OFFICE, SPACE & SITE RENTAL	180,070.00	144,923.73	0.00	35,146.27	80.48
5311	GENERAL OPERATING EXPENSE	44,630.00	46,259.78	0.00	(1,629.78)	103.65
5331	TRAVEL EXPENSE	9,300.00	8,948.47	0.00	351.53	96.22
5332	MILEAGE REIMBURSEMENT	21,020.00	14,098.97	0.00	6,921.03	67.07
5351	UTILITIES	49,500.00	22,336.76	0.00	27,163.24	45.12
5352	FUEL & OIL	707,140.00	271,764.58	0.00	435,375.42	38.43
5901	CONTINGENCIES	57,800.00	0.00	0.00	57,800.00	0.00
	Expenditure Total:	<u>4,708,368.00</u>	<u>3,368,934.94</u>	<u>28,500.02</u>	<u>1,310,933.04</u>	<u>72.15</u>
NET OPERATING		<u>239,354.00</u>	<u>520,574.47</u>	<u>(28,500.02)</u>	<u>(252,720.45)</u>	

CAPITAL ACCOUNT

Revenue

COUNTY OF INYO
Budget to Actuals with Encumbrances by Key/Obj

Ledger: GL

As of 5/18/2016

Object	Description	Budget	Actual	Encumbrance	Balance	%
4066	PTMISEA	460,000.00	467,636.24	0.00	(7,636.24)	101.66
4495	STATE GRANTS - CAPITAL	222,000.00	9,832.46	0.00	212,167.54	4.42
4911	SALES OF FIXED ASSETS	0.00	9,900.00	0.00	(9,900.00)	0.00
Revenue Total:		682,000.00	487,368.70	0.00	194,631.30	71.46
Expenditure						
5640	STRUCTURES & IMPROVEMENTS	532,000.00	499,046.31	10,975.00	21,978.69	95.86
5655	VEHICLES	200,000.00	0.00	0.00	200,000.00	0.00
Expenditure Total:		732,000.00	499,046.31	10,975.00	221,978.69	69.67
NET CAPITAL ACCOUNT		(50,000.00)	(11,677.61)	(10,975.00)	(27,347.39)	
TRANSFERS						
Revenue						
Expenditure						
5798	CAPITAL REPLACEMENT	186,300.00	0.00	0.00	186,300.00	0.00
Expenditure Total:		186,300.00	0.00	0.00	186,300.00	0.00
NET TRANSFERS		0.00	0.00	0.00	0.00	
153299 Total:		3,054.00	508,896.86	(39,475.02)	(466,367.84)	

COUNTY OF INYO

Budget to Actuals with Encumbrances by Key/Income Grouping

Ledger: GL

As Of 5/18/2016

Object	Description	Budget	Actual	Encumbrance	Balance
Key: 153200 - EASTERN SIERRA TRANSIT FUND					
Revenue					
4600	CHARGES FOR CURRENT SERVICES	0.00	0.00	0.00	0.00
4900	OTHER REVENUE	0.00	40.00	0.00	(40.00)
Revenue Total:		0.00	40.00	0.00	(40.00)
Expenditure					
5000	SALARIES & BENEFITS	0.00	0.00	0.00	0.00
5800	OTHER FINANCING USES	0.00	0.00	0.00	0.00
Expenditure Total:		0.00	0.00	0.00	0.00
Key Total:		0.00	40.00	0.00	(40.00)
Key: 153201 - ESTA ADMINISTRATION					
Revenue					
4060	TAXES - SALES	0.00	0.00	0.00	0.00
4350	REV USE OF MONEY & PROPERTY	0.00	5,630.35	0.00	(5,630.35)
4400	AID FROM OTHER GOVT AGENCIES	0.00	0.00	0.00	0.00
4600	CHARGES FOR CURRENT SERVICES	0.00	0.00	0.00	0.00
4900	OTHER REVENUE	0.00	0.00	0.00	0.00
Revenue Total:		0.00	5,630.35	0.00	(5,630.35)
Expenditure					
5000	SALARIES & BENEFITS	0.00	(11.35)	0.00	11.35
5100	SERVICES & SUPPLIES	0.00	0.00	0.00	0.00
5200	INTERNAL CHARGES	0.00	0.00	0.00	0.00
5560	DEBT SERVICE INTEREST	0.00	0.00	0.00	0.00
5600	FIXED ASSETS	0.00	0.00	0.00	0.00
5700	DEPRECIATION	0.00	0.00	0.00	0.00
Expenditure Total:		0.00	(11.35)	0.00	11.35
Key Total:		0.00	5,641.70	0.00	(5,641.70)
Key: 153202 - INYO TRANSIT SERVICE					
Revenue					
4060	TAXES - SALES	311,866.00	204,983.19	0.00	106,882.81
4350	REV USE OF MONEY & PROPERTY	650.00	0.00	0.00	650.00
4400	AID FROM OTHER GOVT AGENCIES	81,440.00	77,365.25	0.00	4,074.75
4600	CHARGES FOR CURRENT SERVICES	51,401.00	47,480.68	0.00	3,920.32
4900	OTHER REVENUE	1,500.00	669.72	0.00	830.28
Revenue Total:		446,857.00	330,498.84	0.00	116,358.16
Expenditure					
5000	SALARIES & BENEFITS	310,260.00	246,185.40	0.00	64,074.60
5100	SERVICES & SUPPLIES	122,800.00	71,900.79	0.00	50,899.21
5200	INTERNAL CHARGES	10,300.00	10,842.00	0.00	(542.00)
5560	DEBT SERVICE INTEREST	0.00	0.00	0.00	0.00
5600	FIXED ASSETS	0.00	0.00	0.00	0.00
5800	OTHER FINANCING USES	12,440.00	0.00	0.00	12,440.00
5900	RESERVES	5,000.00	0.00	0.00	5,000.00
Expenditure Total:		460,800.00	328,928.19	0.00	131,871.81
Key Total:		(13,943.00)	1,570.65	0.00	(15,513.65)
Key: 153203 - MONO TRANSIT SERVICE					
Revenue					
4060	TAXES - SALES	249,613.00	175,348.95	0.00	74,264.05
4350	REV USE OF MONEY & PROPERTY	650.00	0.00	0.00	650.00
4400	AID FROM OTHER GOVT AGENCIES	33,571.00	33,965.52	0.00	(394.52)
4600	CHARGES FOR CURRENT SERVICES	13,494.00	29,136.97	0.00	(15,642.97)

COUNTY OF INYO

Budget to Actuals with Encumbrances by Key/Income Grouping

Ledger: GL

As Of 5/18/2016

Object	Description	Budget	Actual	Encumbrance	Balance
4900	OTHER REVENUE	0.00	10,016.35	0.00	(10,016.35)
Revenue Total:		297,328.00	248,467.79	0.00	48,860.21
Expenditure					
5000	SALARIES & BENEFITS	157,360.00	87,119.48	0.00	70,240.52
5100	SERVICES & SUPPLIES	85,499.00	26,925.62	0.00	58,573.38
5200	INTERNAL CHARGES	6,420.00	5,963.00	0.00	457.00
5600	FIXED ASSETS	0.00	0.00	0.00	0.00
5800	OTHER FINANCING USES	5,200.00	0.00	0.00	5,200.00
5900	RESERVES	2,800.00	0.00	0.00	2,800.00
Expenditure Total:		257,279.00	120,008.10	0.00	137,270.90
Key Total:		40,049.00	128,459.69	0.00	(88,410.69)
Key: 153204 - BISHOP TRANSIT SERVICE					
Revenue					
4060	TAXES - SALES	311,866.00	204,983.28	0.00	106,882.72
4350	REV USE OF MONEY & PROPERTY	650.00	0.00	0.00	650.00
4400	AID FROM OTHER GOVT AGENCIES	81,440.00	89,065.25	0.00	(7,625.25)
4600	CHARGES FOR CURRENT SERVICES	59,783.00	58,538.62	0.00	1,244.38
4900	OTHER REVENUE	1,500.00	819.43	0.00	680.57
Revenue Total:		455,239.00	353,406.58	0.00	101,832.42
Expenditure					
5000	SALARIES & BENEFITS	337,610.00	245,610.10	0.00	91,999.90
5100	SERVICES & SUPPLIES	119,730.00	85,450.07	0.01	34,279.92
5200	INTERNAL CHARGES	11,200.00	10,842.00	0.00	358.00
5560	DEBT SERVICE INTEREST	0.00	0.00	0.00	0.00
5600	FIXED ASSETS	0.00	0.00	0.00	0.00
5800	OTHER FINANCING USES	12,440.00	0.00	0.00	12,440.00
5900	RESERVES	5,000.00	0.00	0.00	5,000.00
Expenditure Total:		485,980.00	341,902.17	0.01	144,077.82
Key Total:		(30,741.00)	11,504.41	(0.01)	(42,245.40)
Key: 153205 - MAMMOTH TRANSIT SERVICE					
Revenue					
4060	TAXES - SALES	348,795.00	232,313.30	0.00	116,481.70
4350	REV USE OF MONEY & PROPERTY	650.00	0.00	0.00	650.00
4400	AID FROM OTHER GOVT AGENCIES	1,035,674.00	712,136.19	0.00	323,537.81
4600	CHARGES FOR CURRENT SERVICES	11,335.00	22,292.23	0.00	(10,957.23)
4900	OTHER REVENUE	1,500.00	1,532.28	0.00	(32.28)
Revenue Total:		1,397,954.00	968,274.00	0.00	429,680.00
Expenditure					
5000	SALARIES & BENEFITS	607,000.00	558,137.62	0.00	48,862.38
5100	SERVICES & SUPPLIES	500,869.00	289,794.77	0.01	211,074.22
5200	INTERNAL CHARGES	22,680.00	22,589.00	0.00	91.00
5600	FIXED ASSETS	222,000.00	5,598.67	0.00	216,401.33
5800	OTHER FINANCING USES	31,220.00	0.00	0.00	31,220.00
5900	RESERVES	12,000.00	0.00	0.00	12,000.00
Expenditure Total:		1,395,769.00	876,120.06	0.01	519,648.93
Key Total:		2,185.00	92,153.94	(0.01)	(89,968.93)
Key: 153206 - 395 ROUTE					
Revenue					
4060	TAXES - SALES	175,454.00	131,400.00	0.00	44,054.00
4400	AID FROM OTHER GOVT AGENCIES	271,051.00	105,008.20	0.00	166,042.80
4600	CHARGES FOR CURRENT SERVICES	170,235.00	175,018.96	0.00	(4,783.96)

COUNTY OF INYO

Budget to Actuals with Encumbrances by Key/Income Grouping

Ledger: GL

As Of 5/18/2016

Object	Description	Budget	Actual	Encumbrance	Balance
4900	OTHER REVENUE	0.00	621.16	0.00	(621.16)
Revenue Total:		616,740.00	412,048.32	0.00	204,691.68
Expenditure					
5000	SALARIES & BENEFITS	322,690.00	245,933.96	0.00	76,756.04
5100	SERVICES & SUPPLIES	273,030.00	138,200.89	0.00	134,829.11
5200	INTERNAL CHARGES	11,020.00	11,294.00	0.00	(274.00)
5600	FIXED ASSETS	0.00	0.00	0.00	0.00
5700	DEPRECIATION	0.00	0.00	0.00	0.00
5900	RESERVES	10,000.00	0.00	0.00	10,000.00
Expenditure Total:		616,740.00	395,428.85	0.00	221,311.15
Key Total:		0.00	16,619.47	0.00	(16,619.47)
Key: 153207 - SPECIALS					
Revenue					
4400	AID FROM OTHER GOVT AGENCIES	0.00	0.00	0.00	0.00
4600	CHARGES FOR CURRENT SERVICES	8,000.00	3,109.65	0.00	4,890.35
Revenue Total:		8,000.00	3,109.65	0.00	4,890.35
Expenditure					
5000	SALARIES & BENEFITS	2,570.00	2,184.63	0.00	385.37
5100	SERVICES & SUPPLIES	3,500.00	244.00	0.00	3,256.00
5200	INTERNAL CHARGES	200.00	181.00	0.00	19.00
5900	RESERVES	0.00	0.00	0.00	0.00
Expenditure Total:		6,270.00	2,609.63	0.00	3,660.37
Key Total:		1,730.00	500.02	0.00	1,229.98
Key: 153208 - COMMUTER VANPOOL					
Revenue					
4600	CHARGES FOR CURRENT SERVICES	0.00	0.00	0.00	0.00
Revenue Total:		0.00	0.00	0.00	0.00
Expenditure					
5000	SALARIES & BENEFITS	0.00	688.24	0.00	(688.24)
5100	SERVICES & SUPPLIES	0.00	0.00	0.00	0.00
5200	INTERNAL CHARGES	0.00	0.00	0.00	0.00
Expenditure Total:		0.00	688.24	0.00	(688.24)
Key Total:		0.00	(688.24)	0.00	688.24
Key: 153209 - REDS MEADOW					
Revenue					
4400	AID FROM OTHER GOVT AGENCIES	0.00	0.00	0.00	0.00
4600	CHARGES FOR CURRENT SERVICES	375,624.00	433,504.82	0.00	(57,880.82)
4900	OTHER REVENUE	500.00	0.00	0.00	500.00
Revenue Total:		376,124.00	433,504.82	0.00	(57,380.82)
Expenditure					
5000	SALARIES & BENEFITS	162,530.00	162,174.60	0.00	355.40
5100	SERVICES & SUPPLIES	179,190.00	200,913.81	0.00	(21,723.81)
5200	INTERNAL CHARGES	6,230.00	7,228.00	0.00	(998.00)
5600	FIXED ASSETS	0.00	0.00	0.00	0.00
5700	DEPRECIATION	0.00	0.00	0.00	0.00
5800	OTHER FINANCING USES	25,000.00	0.00	0.00	25,000.00
5900	RESERVES	3,000.00	0.00	0.00	3,000.00
Expenditure Total:		375,950.00	370,316.41	0.00	5,633.59
Key Total:		174.00	63,188.41	0.00	(63,014.41)
Key: 153210 - MMSA-MAMMOTH MT SKI AREA					

COUNTY OF INYO

Budget to Actuals with Encumbrances by Key/Income Grouping

Ledger: GL

As Of 5/18/2016

Object	Description	Budget	Actual	Encumbrance	Balance
Revenue					
4400	AID FROM OTHER GOVT AGENCIES	0.00	0.00	0.00	0.00
4600	CHARGES FOR CURRENT SERVICES	1,085,430.00	888,686.53	0.00	196,743.47
4900	OTHER REVENUE	0.00	229.27	0.00	(229.27)
Revenue Total:		1,085,430.00	888,915.80	0.00	196,514.20
Expenditure					
5000	SALARIES & BENEFITS	427,610.00	360,026.44	0.00	67,583.56
5100	SERVICES & SUPPLIES	516,800.00	298,926.70	21,600.00	196,273.30
5200	INTERNAL CHARGES	17,420.00	16,715.00	0.00	705.00
5600	FIXED ASSETS	0.00	0.00	0.00	0.00
5700	DEPRECIATION	0.00	0.00	0.00	0.00
5800	OTHER FINANCING USES	100,000.00	0.00	0.00	100,000.00
5900	RESERVES	20,000.00	0.00	0.00	20,000.00
Expenditure Total:		1,081,830.00	675,668.14	21,600.00	384,561.86
Key Total:		3,600.00	213,247.66	(21,600.00)	(188,047.66)

Key: 153299 - EASTERN SIERRA TRANSIT

Revenue

4060	TAXES - SALES	1,555,831.00	1,030,028.72	0.00	525,802.28
4350	REV USE OF MONEY & PROPERTY	2,600.00	9,054.80	0.00	(6,454.80)
4400	AID FROM OTHER GOVT AGENCIES	2,238,385.00	1,626,285.94	0.00	612,099.06
4600	CHARGES FOR CURRENT SERVICES	1,827,906.00	1,697,026.49	0.00	130,879.51
4800	OTHER FINANCING SOURCES	0.00	0.00	0.00	0.00
4900	OTHER REVENUE	5,000.00	14,482.16	0.00	(9,482.16)
Revenue Total:		5,629,722.00	4,376,878.11	0.00	1,252,843.89

Expenditure

5000	SALARIES & BENEFITS	2,487,600.00	2,022,666.72	0.00	464,933.28
5100	SERVICES & SUPPLIES	2,072,078.00	1,255,915.22	28,500.02	787,662.76
5200	INTERNAL CHARGES	90,890.00	90,353.00	0.00	537.00
5560	DEBT SERVICE INTEREST	0.00	0.00	0.00	0.00
5600	FIXED ASSETS	732,000.00	499,046.31	10,975.00	221,978.69
5700	DEPRECIATION	0.00	0.00	0.00	0.00
5800	OTHER FINANCING USES	186,300.00	0.00	0.00	186,300.00
5900	RESERVES	57,800.00	0.00	0.00	57,800.00
Expenditure Total:		5,626,668.00	3,867,981.25	39,475.02	1,719,211.73
Key Total:		3,054.00	508,896.86	(39,475.02)	(466,367.84)

Key: 153211 - ESTA ACCUMULATED CAPITAL OUT

Revenue

4350	REV USE OF MONEY & PROPERTY	0.00	1,405.07	0.00	(1,405.07)
4600	CHARGES FOR CURRENT SERVICES	0.00	0.00	0.00	0.00
4800	OTHER FINANCING SOURCES	0.00	0.00	0.00	0.00
Revenue Total:		0.00	1,405.07	0.00	(1,405.07)
Key Total:		0.00	1,405.07	0.00	(1,405.07)

Key: 153212 - ESTA GENERAL RESERVE

Revenue

4350	REV USE OF MONEY & PROPERTY	0.00	1,531.15	0.00	(1,531.15)
4800	OTHER FINANCING SOURCES	0.00	0.00	0.00	0.00
Revenue Total:		0.00	1,531.15	0.00	(1,531.15)
Key Total:		0.00	1,531.15	0.00	(1,531.15)

Key: 153213 - ESTA-BUDGET STABILIZATION RESER

Revenue

COUNTY OF INYO

Budget to Actuals with Encumbrances by Key/Income Grouping

Ledger: GL

As Of 5/18/2016

<u>Object</u>	<u>Description</u>	<u>Budget</u>	<u>Actual</u>	<u>Encumbrance</u>	<u>Balance</u>
4350	REV USE OF MONEY & PROPERTY	0.00	612.16	0.00	(612.16)
4800	OTHER FINANCING SOURCES	0.00	0.00	0.00	0.00
Revenue Total:		0.00	612.16	0.00	(612.16)
Key Total:		0.00	612.16	0.00	(612.16)
Key: 612502 - SRTP TRANSPORT PLAN					
Revenue					
4400	AID FROM OTHER GOVT AGENCIES	94,620.00	54,818.20	0.00	39,801.80
Revenue Total:		94,620.00	54,818.20	0.00	39,801.80
Expenditure					
5100	SERVICES & SUPPLIES	94,620.00	77,124.88	0.00	17,495.12
Expenditure Total:		94,620.00	77,124.88	0.00	17,495.12
Key Total:		0.00	(22,306.68)	0.00	22,306.68
Key: 612490 - ACIS-AUTOMATED CUSTOMER IS					
Expenditure					
5000	SALARIES & BENEFITS	0.00	0.00	0.00	0.00
Expenditure Total:		0.00	0.00	0.00	0.00
Key Total:		0.00	0.00	0.00	0.00
Key: 612491 - NIGHT RIDER					
Revenue					
4600	CHARGES FOR CURRENT SERVICES	0.00	0.00	0.00	0.00
Revenue Total:		0.00	0.00	0.00	0.00
Expenditure					
5000	SALARIES & BENEFITS	0.00	0.00	0.00	0.00
Expenditure Total:		0.00	0.00	0.00	0.00
Key Total:		0.00	0.00	0.00	0.00
Key: 612493 - JARC-LONE PINE/BISHOP					
Revenue					
4060	TAXES - SALES	65,263.00	48,600.00	0.00	16,663.00
4400	AID FROM OTHER GOVT AGENCIES	65,264.00	22,822.46	0.00	42,441.54
4600	CHARGES FOR CURRENT SERVICES	24,503.00	21,733.67	0.00	2,769.33
4900	OTHER REVENUE	0.00	553.95	0.00	(553.95)
Revenue Total:		155,030.00	93,710.08	0.00	61,319.92
Expenditure					
5000	SALARIES & BENEFITS	80,780.00	62,847.57	0.00	17,932.43
5100	SERVICES & SUPPLIES	71,540.00	21,810.28	0.00	49,729.72
5200	INTERNAL CHARGES	2,710.00	2,711.00	0.00	(1.00)
Expenditure Total:		155,030.00	87,368.85	0.00	67,661.15
Key Total:		0.00	6,341.23	0.00	(6,341.23)
Key: 612494 - JARC-MAMMOTH EXPRESS					
Revenue					
4060	TAXES - SALES	42,974.00	32,400.00	0.00	10,574.00
4400	AID FROM OTHER GOVT AGENCIES	76,975.00	29,319.32	0.00	47,655.68
4600	CHARGES FOR CURRENT SERVICES	28,101.00	17,524.36	0.00	10,576.64
Revenue Total:		148,050.00	79,243.68	0.00	68,806.32
Expenditure					
5000	SALARIES & BENEFITS	73,410.00	47,187.33	0.00	26,222.67
5100	SERVICES & SUPPLIES	71,930.00	17,972.79	0.00	53,957.21
5200	INTERNAL CHARGES	2,710.00	1,988.00	0.00	722.00
Expenditure Total:		148,050.00	67,148.12	0.00	80,901.88

COUNTY OF INYO

Budget to Actuals with Encumbrances by Key/Income Grouping

Ledger: GL

As Of 5/18/2016

Object	Description	Budget	Actual	Encumbrance	Balance
Key Total:		0.00	12,095.56	0.00	(12,095.56)
Key: 612496 - MONO COUNTY BUS SHELTERS					
Revenue					
4400	AID FROM OTHER GOVT AGENCIES	0.00	0.00	0.00	0.00
Revenue Total:		0.00	0.00	0.00	0.00
Expenditure					
5000	SALARIES & BENEFITS	0.00	0.00	0.00	0.00
5100	SERVICES & SUPPLIES	0.00	0.00	0.00	0.00
Expenditure Total:		0.00	0.00	0.00	0.00
Key Total:		0.00	0.00	0.00	0.00
Key: 612497 - GOOGLE TRANSIT PHASE 2					
Revenue					
4400	AID FROM OTHER GOVT AGENCIES	12,500.00	13,630.87	0.00	(1,130.87)
4600	CHARGES FOR CURRENT SERVICES	0.00	0.00	0.00	0.00
Revenue Total:		12,500.00	13,630.87	0.00	(1,130.87)
Expenditure					
5000	SALARIES & BENEFITS	1,570.00	957.46	0.00	612.54
5100	SERVICES & SUPPLIES	10,930.00	12,700.00	6,900.00	(8,670.00)
Expenditure Total:		12,500.00	13,657.46	6,900.00	(8,057.46)
Key Total:		0.00	(26.59)	(6,900.00)	6,926.59
Key: 612498 - CAPP-CLEAN AIR PROJECT PROGRAM					
Revenue					
4400	AID FROM OTHER GOVT AGENCIES	0.00	0.00	0.00	0.00
Revenue Total:		0.00	0.00	0.00	0.00
Expenditure					
5000	SALARIES & BENEFITS	0.00	0.00	0.00	0.00
5100	SERVICES & SUPPLIES	0.00	0.00	0.00	0.00
Expenditure Total:		0.00	0.00	0.00	0.00
Key Total:		0.00	0.00	0.00	0.00
Key: 612499 - MOBILITY MANAGEMENT 14					
Revenue					
4400	AID FROM OTHER GOVT AGENCIES	0.00	0.00	0.00	0.00
Revenue Total:		0.00	0.00	0.00	0.00
Expenditure					
5100	SERVICES & SUPPLIES	0.00	0.00	0.00	0.00
Expenditure Total:		0.00	0.00	0.00	0.00
Key Total:		0.00	0.00	0.00	0.00
Key: 612489 - NON-EMERGENCY TRAN REIM					
Revenue					
4400	AID FROM OTHER GOVT AGENCIES	25,850.00	15,707.28	0.00	10,142.72
Revenue Total:		25,850.00	15,707.28	0.00	10,142.72
Expenditure					
5000	SALARIES & BENEFITS	4,210.00	3,625.24	0.00	584.76
5100	SERVICES & SUPPLIES	21,640.00	13,950.62	0.00	7,689.38
Expenditure Total:		25,850.00	17,575.86	0.00	8,274.14
Key Total:		0.00	(1,868.58)	0.00	1,868.58
Key: 612503 - BISHOP YARD-ESTA					
Revenue					
4060	TAXES - SALES	50,000.00	0.00	0.00	50,000.00

COUNTY OF INYO

Budget to Actuals with Encumbrances by Key/Income Grouping

Ledger: GL

As Of 5/18/2016

Object	Description	Budget	Actual	Encumbrance	Balance
4350	REV USE OF MONEY & PROPERTY	0.00	(123.93)	0.00	123.93
4400	AID FROM OTHER GOVT AGENCIES	460,000.00	472,447.40	0.00	(12,447.40)
4900	OTHER REVENUE	0.00	0.00	0.00	0.00
Revenue Total:		510,000.00	472,323.47	0.00	37,676.53
Expenditure					
5100	SERVICES & SUPPLIES	0.00	0.00	0.00	0.00
5600	FIXED ASSETS	510,000.00	493,447.64	10,975.00	5,577.36
Expenditure Total:		510,000.00	493,447.64	10,975.00	5,577.36
Key Total:		0.00	(21,124.17)	(10,975.00)	32,099.17

**COUNTY OF INYO
UNDESIGNATED FUND BALANCES**

AS OF 06/30/2016

	Claim on Cash 1000	Accounts Receivable 1100,1105,1160	Loans Receivable 1140	Prepaid Expenses 1200	Accounts Payable 2000	Loans Payable 2140	Deferred Revenue 2200	Computed Fund Balance	Encumbrances	Fund Balance Undesignated
ESTA - EASTERN SIERRA TRANSIT AUTHORI										
1532 EASTERN SIERRA TRANSIT	2,546,844	21,984	86,074		7,205			2,647,697	21,600	2,626,097
1533 ESTA ACCUMULATED	460,122							460,122		460,122
1534 ESTA GENERAL RESERVE	501,603							501,603		501,603
1535 ESTA BUDGET STAB	200,641							200,641		200,641
6809 SRTP TRANSPORT PLAN	2,693					25,000		(22,307)		(22,307)
6813 JARC-LONE PINE/BISHOP	19,860					27,000		(7,140)		(7,140)
6814 JARC-MAMMOTH EXPRESS	22,859					7,000		15,859		15,859
6817 GOOGLE TRANSIT PHASE 2	16,244					17,000		(756)	6,900	(7,656)
6818 CAPP-CLEAN AIR PROJECT	2,923							2,923		2,923
6819 MOBILITY MANAGEMENT 14	2,227							2,227		2,227
6820 NON-EMERGENCY TRAN REIM	5,497					7,500		(2,003)		(2,003)
6821 BISHOP YARD-ESTA	1,832					2,574		(742)	10,975	(11,717)
ESTA Totals	3,783,345	21,984	86,074		7,205	86,074		3,798,124	39,475	3,758,649
Grand Totals	3,783,345	21,984	86,074		7,205	86,074		3,798,124	39,475	3,758,649

STAFF REPORT

Subject: Fiscal Year End Projection

Initiated by: John Helm, Executive Director

The year-end financial report projection for FY 2015/16 is included on the following pages. The projection anticipates that net total revenues will exceed expenses by \$289,110, which is \$286,056 better than budget. The most significant contributor to the positive financial performance is the continued low fuel costs. Fuel savings alone are projected to result in more than \$300,000 in operational savings. Some of these savings, which are also manifested in labor and maintenance are a result of operating fewer than budgeted service hours this month related to the early termination of the Red Line service this spring. The revenue reductions from this service reduction were offset by increases in Reds Meadow shuttle revenue last July and August, which moderated any reduction in the Services and Fees line item. The line-item detail of the year-end projections are listed on the following pages.

FY 2015/16 YEAR-END PROJECTION

OPERATING		BUDGET	ACTUAL thru 05.18.16	PROJECTED FISCAL YEAR TOTAL	VARIANCE FROM BUDGET
Revenue					
4061	LOCAL TRANSPORTATION TAX	1,232,955	961,183	1,233,000	45
4065	STATE TRANSIT ASST	322,876	68,846	106,000	-216,876
4301	INTEREST FROM TREASURY	2,600	9,055	10,000	7,400
4499	STATE OTHER	111,620	66,076	89,300	-22,320
4555	FEDERAL GRANTS	594,951	339,541	520,000	-74,951
4599	OTHER AGENCIES	849,814	743,200	850,000	186
4819	SERVICES & FEES	1,827,906	1,697,026	1,827,830	-76
4959	MISCELLANEOUS REVENUE	5,000	4,582	5,000	0
Revenue Total:		4,947,722	3,889,509	4,641,130	-306,592
Expenditure					
5001	SALARIED EMPLOYEES	1,163,600	956,867	1,100,000	-63,600
5003	OVERTIME	32,230	36,276	38,000	5,770
5005	HOLIDAY OVERTIME	101,606	87,563	101,000	-606
5012	PART TIME EMPLOYEES	431,128	358,709	390,000	-41,128
5021	RETIREMENT & SOCIAL SECURITY	40,306	34,300	40,000	-306
5022	PERS RETIREMENT	229,100	180,918	228,000	-1,100
5031	MEDICAL INSURANCE	298,940	245,514	295,000	-3,940
5041		0	0	0	0
5043	OTHER BENEFITS	32,290	21,111	32,000	-290
5045	COMPENSATED ABSENCE EXPENSE	143,500	95,941	138,000	-5,500
5047	EMPLOYEE INCENTIVES	6,500	2,181	3,000	-3,500
5111	CLOTHING	8,400	3,286	6,500	-1,900
5152	WORKERS COMPENSATION	90,890	90,353	90,400	-490
5154	UNEMPLOYMENT INSURANCE	45,000	22,302	41,000	-4,000
5158	INSURANCE PREMIUM	167,000	167,701	167,700	700
5171	MAINTENANCE OF EQUIPMENT	544,860	326,509	490,000	-54,860
5173	MAINTENANCE OF EQUIPMENT-MATER	22,300	12,807	15,000	-7,300
5191	MAINTENANCE OF STRUCTURES	18,000	2,269	5,000	-13,000
5211	MEMBERSHIPS	2,142	610	1,200	-942
5232	OFFICE & OTHER EQUIP < \$5,000	11,100	4,765	10,000	-1,100
5238	OFFICE SUPPLIES	9,186	6,288	7,500	-1,686
5253	ACCOUNTING & AUDITING SERVICE	40,550	36,100	40,600	50
5254	AUDITING SERVICE	8,360	0	0	-8,360
5260	HEALTH - EMPLOYEE PHYSICALS	6,400	4,689	6,000	-400
5263	ADVERTISING	36,600	29,629	36,000	-600
5265	PROFESSIONAL & SPECIAL SERVICE	148,920	133,914	150,000	1,080
5281		0	0		0
5291	OFFICE, SPACE & SITE RENTAL	180,070	144,924	180,000	-70
5311	GENERAL OPERATING EXPENSE	44,630	46,260	49,000	4,370
5331	TRAVEL EXPENSE	9,300	8,948	9,300	0
5332	MILEAGE REIMBURSEMENT	21,020	14,099	21,020	0
5351	UTILITIES	49,500	22,337	40,000	-9,500
5352	FUEL & OIL	707,140	271,765	400,000	-307,140
5901	CONTINGENCIES	57,800	0		-57,800
Expenditure Total:		4,708,368	3,368,935	4,131,220	-577,148
NET OPERATING REVENUES MINUS EXPENDITURES		239,354	520,574	509,910	270,556

CAPITAL ACCOUNT					
Revenue					
4066	PTMISEA	460,000	467,636	468,000	8,000
4495	STATE GRANTS - CAPITAL	222,000	9,832	210,000	-12,000
4911	SALES OF FIXED ASSETS	0	9,900	10,000	10,000
Revenue Total:		682,000	487,369	688,000	6,000
Expenditure					
5640	STRUCTURES & IMPROVEMENTS	532,000	499,046	520,000	-12,000
5655	VEHICLES	200,000	0	202,500	2,500
Expenditure Total:		732,000	499,046	722,500	-9,500
NET	CAPITAL ACCOUNT	-50,000	-11,678	-34,500	15,500
TRANSFERS					
Revenue					
Expenditure					
5798	CAPITAL REPLACEMENT	186,300	0	186,300	0
Expenditure Total:		186,300	0	186,300	0
NET	TRANSFERS	0	0		
153299	Total:	3,054	508,897	289,110	286,056

MONTHLY REPORT

APRIL 2016

	Apr-16	Mar-16	Percent Change	Apr-15	Percent Change
PASSENGERS					
Adult	52,383	103,752	-49.5%	42,329	23.8%
Senior	1,811	1,788	1.3%	1,536	17.9%
Disabled	961	1,176	-18.3%	1,248	-23.0%
Wheelchair	381	376	1.3%	366	4.1%
Child	8,380	13,183	-36.4%	9,279	-9.7%
Child under 5	250	329	-24.0%	277	-9.7%
Total Passengers	64,166	120,604	-46.8%	55,035	16.6%
FARES	\$30,987.82	\$36,022.55	-14.0%	\$27,494.25	12.7%
SERVICE MILES	75,347	87,086	-13.5%	66,268	13.7%
SERVICE HOURS	4,650	5,485	-15.2%	3,967	17.2%
PASSENGER PER HOUR	13.80	21.99	-37.2%	13.87	-0.5%

RIDERSHIP COMPARISON

REPORT MONTH - THIS YEAR/LAST YEAR					FISCAL YEAR TO DATE		
Route	Apr-16	Apr-15	Variance	% Change	FY 15/16	FY 14/15	% Change
Mammoth Express	337	226	111	49.1%	3,391	2,963	14.4%
Lone Pine to Bishop	364	284	80	28.2%	3,399	4,546	-25.2%
Lone Pine DAR	277	304	-27	-8.9%	2,601	3,588	-27.5%
Tecopa	8	8	0	0.0%	118	155	-23.9%
Walker DAR	205	210	-5	-2.4%	2,042	2,001	2.0%
Bridgeport to G'Ville	40	59	-19	-32.2%	398	525	-24.2%
Benton to Bishop	29	69	-40	-58.0%	295	1,053	-72.0%
Bishop DAR	3,418	3,560	-142	-4.0%	35,056	39,466	-11.2%
Nite Rider	351	266	85	32.0%	3,396	4,048	-16.1%
Mammoth FR	17,140	17,927	-787	-4.4%	335,373	366,949	-8.6%
Mammoth DAR	197	265	-68	-25.7%	2,541	2,818	-9.8%
Reno	396	375	21	5.6%	5,037	5,451	-7.6%
Lancaster	379	373	6	1.6%	3,852	4,943	-22.1%
MMSA	40,926	31,109	9,817	31.6%	489,117	355,609	37.5%
June Lake Shuttle	99	0	99	N/A	2,416	2,804	-13.8%
Total	64,166	55,035	9,131	16.6%	1,030,924	962,536	7.1%

PASSENGERS PER SERVICE HOUR

REPORT MONTH - THIS YEAR/LAST YEAR					FISCAL YEAR TO DATE		
Route	Apr-16	Apr-15	% Change	Pax Miles / Svc Hr	FY 15/16	FY 14/15	% Change
Mammoth Express	3.19	3.29	-3.1%		2.83	3.47	-18.5%
Lone Pine to Bishop	3.26	2.41	35.4%		2.96	3.36	-11.8%
Lone Pine DAR	1.88	1.97	-4.5%		1.78	2.05	-13.0%
Tecopa	0.74	1.45	-49.2%		1.07	1.32	-18.7%
Walker DAR	1.70	1.56	9.5%		1.62	1.33	22.2%
Bridgeport to G'Ville	1.45	2.09	-30.6%		1.49	1.66	-10.3%
Benton to Bishop	2.08	4.48	-53.6%		2.21	4.77	-53.6%
Bishop DAR	3.76	4.06	-7.4%		3.98	3.93	1.1%
Nite Rider	4.57	4.43	3.2%		5.02	5.00	0.4%
Mammoth FR	18.02	18.05	-0.1%		25.18	23.19	8.6%
Mammoth DAR	1.04	1.49	-30.0%		1.34	1.30	3.3%
Reno	1.75	1.74	0.4%	152.60	2.23	2.10	6.1%
Lancaster	2.75	2.67	3.0%	260.84	2.84	3.01	-5.8%
MMSA	25.79	31.77	-18.8%		45.42	41.19	10.3%
June Lake Shuttle	2.77	N/A	N/A		5.34	9.00	-40.7%
Total	13.80	13.87	-0.5%		21.04	18.23	15.4%

Route	Fares	Adults	Snr	Dis	W/C	Child	Free	Total Pax	Yd Hrs	Svc Hours	Yd Mi	SVC MILES	AVG FARE	REV /SVC MILE	PAX / SVC HR	MI / SVC HR	PAX / SVC MI
Apr-16																	
Mammoth Express	\$1,982.50	273	27	11	1	15	10	337	138	106	4,729	4,579	5.88	.43	3.19	44.7	0.07
Lone Pine to Bishop	\$1,834.97	238	56	38	10	11	11	364	130	112	4,970	4,810	5.04	.38	3.26	44.5	0.08
Lone Pine DAR	\$711.60	12	150	48	18	49	0	277	154	147	1,463	1,462	2.57	.49	1.88	10.0	0.19
Tecopa	\$39.00	0	8	0	0	0	0	8	11	11	181	181	4.88	.22	.74	16.7	0.04
Walker DAR	\$566.60	0	31	172	0	2	0	205	128	120	945	819	2.76	.69	1.70	7.9	0.25
Bridgeport to G'Ville	\$309.00	8	32	0	0	0	0	40	32	28	857	575	7.73	.54	1.45	31.1	0.07
Benton to Bishop	\$134.00	8	4	12	0	0	5	29	29	14	1,290	644	4.62	.21	2.08	92.4	0.05
Bishop DAR	\$7,810.90	942	1,306	527	333	135	175	3,418	990	910	10,852	9,856	2.29	.79	3.76	11.9	0.35
Nite Rider	\$1,327.80	227	35	50	14	2	23	351	79	77	1,095	1,055	3.78	1.26	4.57	14.3	0.33
Mammoth FR	\$0.00	13,803	0	0	0	3,337	0	17,140	983	951	14,034	13,587	N/A	N/A	18.02	14.8	1.26
Mammoth DAR	\$505.20	120	30	17	0	10	20	197	193	189	889	836	2.56	.60	1.04	4.7	0.24
Reno	\$7,677.25	265	69	43	4	15	0	396	252	226	9,836	9,385	19.39	.82	1.75	43.5	0.04
Lancaster	\$5,538.00	257	63	41	1	11	6	379	153	138	6,579	6,496	14.61	.85	2.75	47.8	0.06
MMSA	\$0.00	36,131	0	2	0	4,793	0	40,926	1,665	1,587	21,162	20,095	N/A	N/A	25.79	13.3	2.04
June Lake Shuttle	\$2,551.00	99	0	0	0	0	0	99	41	36	1,057	967	25.77	2.64	2.77	29.6	0.10
Total	\$30,987.82	52,383	1,811	961	381	8,380	250	64,166	4,979	4,650	79,939	75,347	.48	.41	13.80	17.2	0.85
Apr-15																	
Mammoth Express	\$1,239.40	171	24	8	0	15	8	226	96	69	3,140	3,040	5.48	.41	3.29	45.7	0.07
Lone Pine to Bishop	\$1,370.60	169	44	50	0	19	2	284	139	118	5,284	5,132	4.83	.27	2.41	44.8	0.06
Lone Pine DAR	\$725.20	49	93	54	20	82	6	304	161	154	1,121	1,112	2.39	.65	1.97	7.3	0.27
Tecopa	\$39.00	0	8	0	0	0	0	8	6	6	89	89	4.88	.44	1.45	16.2	0.09
Walker DAR	\$576.90	0	7	189	0	14	0	210	144	135	1,217	1,077	2.75	.54	1.56	9.0	0.19
Bridgeport to G'Ville	\$409.60	9	50	0	0	0	0	59	33	28	877	615	6.94	.67	2.09	31.0	0.10
Benton to Bishop	\$350.50	40	8	9	0	4	8	69	30	15	1,298	682	5.08	.51	4.48	84.2	0.10
Bishop DAR	\$7,736.10	936	1,146	735	328	206	209	3,560	955	878	10,015	9,119	2.17	.85	4.06	11.4	0.39
Nite Rider	\$1,052.40	183	20	42	18	0	3	266	61	60	886	880	3.96	1.20	4.43	14.8	0.30
Mammoth FR	\$0.00	13,601	0	2	0	4,324	0	17,927	1,027	993	14,848	14,358	N/A	N/A	18.05	14.9	1.25
Mammoth DAR	\$693.30	132	19	70	0	17	27	265	180	178	938	850	2.62	.82	1.49	5.3	0.31
Reno	\$7,822.75	250	61	31	0	30	3	375	238	215	9,624	9,365	20.86	.84	1.74	44.7	0.04
Lancaster	\$5,478.50	243	56	58	0	5	11	373	158	139	6,628	6,513	14.69	.84	2.67	47.5	0.06
MMSA	\$0.00	26,546	0	0	0	4,563	0	31,109	1,026	979	14,116	13,436	N/A	N/A	31.77	14.4	2.32
June Lake Shuttle	\$0.00	0	0	0	0	0	0	0	0	0	0	0	N/A	N/A	N/A	N/A	N/A
Total	\$27,494.25	42,329	1,536	1,248	366	9,279	277	55,035	4,253	3,967	70,081	66,268	.50	.41	13.87	17.7	0.83

VARIANCE BY ROUTE (RAW NUMBERS) – April 2016 to April 2015																	
ROUTES	FARES	ADULTS	SNR	DIS	W/C	CHIL D	FREE	TOTAL PAX	YD HOURS	SVC HOURS	YD MILES	SVC MILES	AVG FARE	REV/SVC MILE	PAX / SVC HR	MI / SVC HR	PAX / SVC MI
Mammoth Express	\$743.10	102	3	3	1	0	2	111	42	37	1589	1539	0.40	0.03	-0.10	-0.96	0.00
Lone Pine to Bishop	\$464.37	69	12	-12	10	-8	9	80	-9	-6	-314	-322	0.22	0.11	0.85	-0.28	0.02
Lone Pine DAR	-\$13.60	-37	57	-6	-2	-33	-6	-27	-7	-7	342	350	0.18	-0.17	-0.09	2.67	-0.08
Tecopa	\$0.00	0	0	0	0	0	0	0	6	5	92	92	0.00	-0.22	-0.72	0.53	-0.05
Walker DAR	-\$10.30	0	24	-17	0	-12	0	-5	-16	-15	-272	-258	0.02	0.16	0.15	-1.16	0.06
Bridgeport to G'Ville	-\$100.60	-1	-18	0	0	0	0	-19	0	-1	-20	-40	0.78	-0.13	-0.64	0.03	-0.03
Benton to Bishop	-\$216.50	-32	-4	3	0	-4	-3	-40	-1	-1	-8	-38	-0.46	-0.31	-2.40	8.17	-0.06
Bishop DAR	\$74.80	6	160	-208	5	-71	-34	-142	35	32	837	737	0.11	-0.06	-0.30	0.52	-0.04
Nite Rider	\$275.40	44	15	8	-4	2	20	85	18	17	209	175	-0.17	0.06	0.14	-0.50	0.03
Mammoth FR	\$0.00	202	0	-2	0	-987	0	-787	-43	-42	-814	-771	N/A	N/A	-0.03	-0.19	0.01
Mammoth DAR	-\$188.10	-12	11	-53	0	-7	-7	-68	13	11	-49	-14	-0.05	-0.21	-0.45	-0.57	-0.08
Reno	-\$145.50	15	8	12	4	-15	-3	21	15	11	212	20	-1.47	-0.02	0.01	-1.25	0.00
Lancaster	\$59.50	14	7	-17	1	6	-5	6	-5	-2	-49	-17	-0.08	0.01	0.08	0.28	0.00
MMSA	\$0.00	9,585	0	2	0	230	0	9,817	639	608	7,046	6,659	N/A	N/A	-5.98	-1.08	-0.28
June Lake Shuttle	\$2,551.00	99	0	0	0	0	0	99	41	36	1057	967	N/A	N/A	N/A	N/A	N/A

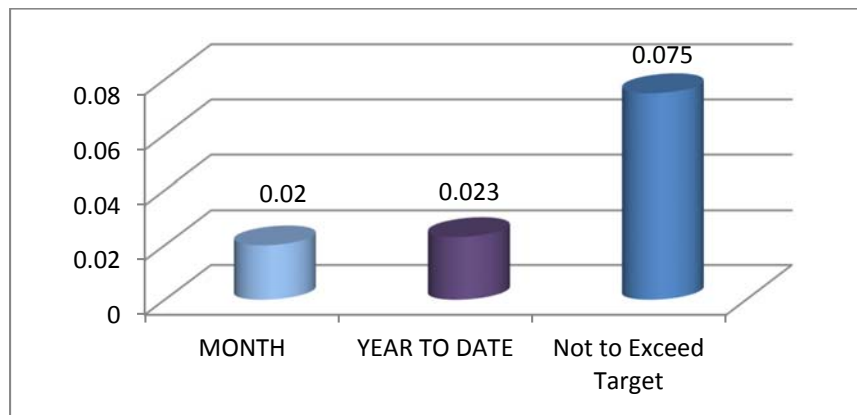
VARIANCE BY ROUTE (PERCENTAGE) – April 2016 to April 2015																	
Route	Fares	Adults	Snr	Dis	W/C	Child	Free	Total Pax	Yd Hrs	Total Svc Hours	Yd Mi	TOT SVC MILES	AVG FARE	REV/SVC MILE	PAX / SVC HR	MI / SVC HR	PAX / SVC MI
Mammoth Express	60%	60%	13%	38%		0%	25%	49%	44%	54%	51%	51%	7%	6%	-3%	-2%	-1%
Lone Pine to Bishop	34%	41%	27%	-24%		-42%	450%	28%	-6%	-5%	-6%	-6%	4%	43%	35%	-1%	37%
Lone Pine DAR	-2%	-76%	61%	-11%	-10%	-40%	-100%	-9%	-5%	-5%	31%	31%	8%	-25%	-5%	37%	-31%
Tecopa	0.0%		0.0%					0.0%	97.1%	97.0%	103.4%	103.4%	0.0%	-50.8%	-49.2%	3.2%	-50.8%
Walker DAR	-2%		343%	-9%		-86%		-2%	-11%	-11%	-22%	-24%	1%	29%	10%	-13%	28%
Bridgeport to G'Ville	-25%	-11%	-36%					-32%	-1%	-2%	-2%	-7%	11%	-19%	-31%	0%	-27%
Benton to Bishop	-62%	-80%	-50%	33%		-100%	-38%	-58%	-4%	-9%	-1%	-6%	-9%	-60%	-54%	10%	-55%
Bishop DAR	1.0%	0.6%	14.0%	-28.3%	1.5%	-34.5%	-16.3%	-4.0%	3.7%	3.6%	8.4%	8.1%	5.2%	-6.6%	-7.4%	4.6%	-11.2%
Nite Rider	26%	24%	75%	19%	-22%		667%	32%	29%	28%	24%	20%	-4%	5%	3%	-3%	10%
Mammoth FR		1%		-100%		-23%		-4%	-4%	-4%	-5%	-5%	N/A	N/A	0%	-1%	1%
Mammoth DAR	-27%	-9%	58%	-76%		-41%	-26%	-26%	7%	6%	-5%	-2%	-2%	-26%	-30%	-11%	-24%
Reno	-2%	6%	13%	39%		-50%	-100%	6%	6%	5%	2%	0%	-7%	-2%	0%	-3%	5%
Lancaster	1%	6%	13%	-29%		120%	-45%	2%	-3%	-1%	-1%	0%	-1%	1%	3%	1%	2%
MMSA		36%				5%		32%	62%	62%	50%	50%	N/A	N/A	-19%	-8%	-12%
June Lake Shuttle													N/A	N/A	N/A	N/A	N/A

Comments

There were two comments received during the month of April 2016.

- April 4th – Individual called to say that the Red Line driver had pulled out in front of him on Old Mammoth Road. Driver was counseled about defensive driving and using mirrors and turn signals when departing from bus stops.
- April 16th - Passenger wrote to compliment the Gray Line driver who went out of her way to help the passenger retrieve her jacket.

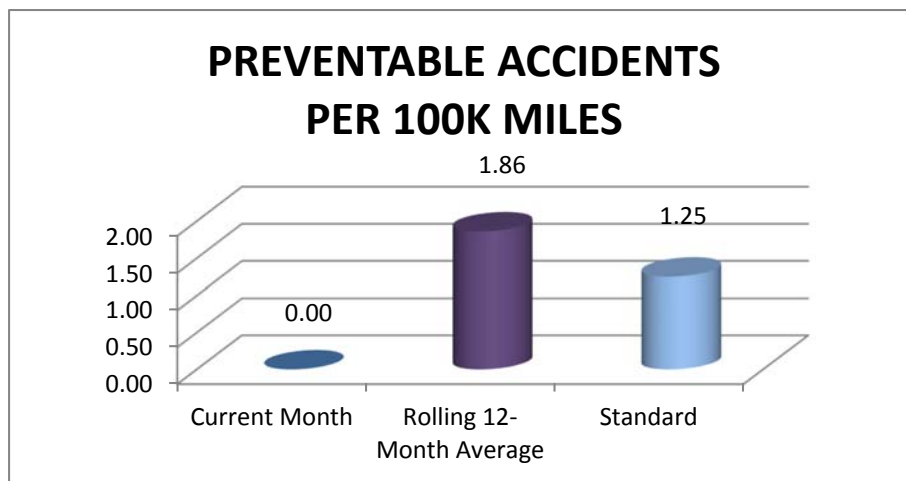
COMPLAINTS PER 1,000 PASSENGERS



Accident/Incidents

There were no preventable accidents in April 2016.

PREVENTABLE ACCIDENTS PER 100K MILES

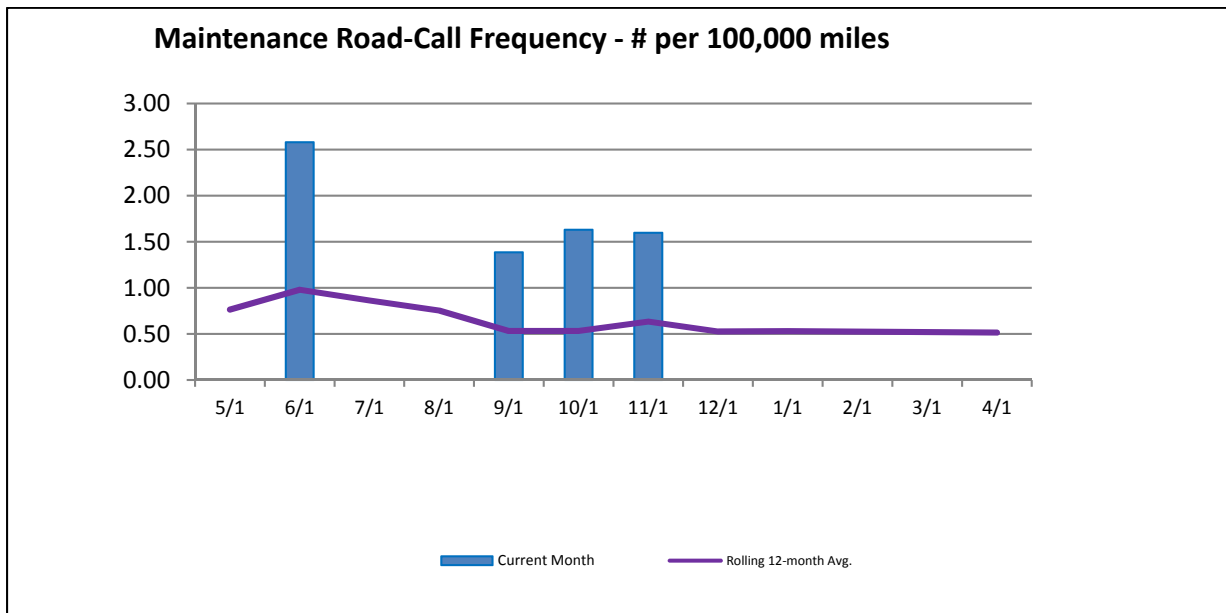


Missed Runs

There were no missed run in April 2016.

Road Call Frequency

There were no Road Calls during the month of April 2016. The rolling 12-month road call frequency is 0.51 per 100,000 miles traveled.



Bishop Area Dial-A-Ride Wait Times

Wait times for the Bishop Area Dial-A-Ride (Mon. through Fri., 7:00 a.m. – 6:00 p.m.)

APRIL 2016

		Percent	Goal
IMMEDIATE RESPONSE TRIPS			
Total Trips:	1,741	68% of trips	
Average Wait Time (min.):	13		< 20 minutes
# > 30 minute wait:	113	6.5%	< 5%
ADVANCE RESERVATION TRIPS			
Total Trips:	844	32% of trips	
On Time Trips (± 10 min.)	674	80%	
TOTAL SCHEDULED TRIPS			
No-Shows	202 / 94	7.3% / 3.4%	Incl / Excl Ckpts
Cancellations	63	2.3%	

